

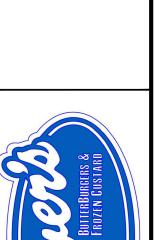


575 W LAYTON AVE MILWAUKEE, WI 53207



Hearing Impaired TDD (800) 542-2289 www.DiggersHotline.com

Kimley » Horn





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ORIGINAL ISSUE: 02/08/2023 KHA PROJECT NO. 168974004 SHEET NUMBER

ANDREW

HEINEN 062-056580

Date of Expiration: 11-30-23

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# **UTILITY AND GOVERNING AGENCY CONTACTS**

PLAN COMMISSION CITY OF MILWAUKEE 809 NORTH BROADWAY, 2ND FLOOR MILWAUKEE, WI 53202 TEL: (414) 286-5800 CONTACT: STEPHANIE BLOOMINGDALE

PUBLIC WORKS DEPARTMENT CITY OF MILWAUKEE 841 NORTH BROADWAY, ROOM 501 MILWAUKEE, WI 53202 TEL: (515) 286-2489 CONTACT: JERREL KRUSCHKE, P.E.

FIRE DEPARTMENT CITY OF MILWAUKEE 711 W WELLS STREET MILWAUKEE. WI 53233 TEL: (414) 286-5213 CONTACT: AARON LIPSKI, FIRE CHIEF

MILWAUKEE WATER WORKS 841 N. BROADWAY, 4TH FLOOR MILWAUKEE, WI 53202 TEL: (414) 286-2830

MILWAUKEE METROPOLITAN SEWERAGE DISTRICT (MMSD) 260 W. SÈEBOTH STREET MILWAUKEE, WI 53204 TEL: (414) 272-5100

POWER COMPANY WE ENERGIES 231 W. MICHIGAN STREET MILWAUKEE, WI 53203 TEL: (800) 242-9137

NATURAL GAS COMPANY WE ENERGIES 231 W. MICHIGAN STREET MILWAUKEE, WI 53203 TEL: (800) 242-9137

PROJECT TEAM

<u>DEVELOPER</u> NEO GEN, INC. 3510 N. OAKLAND AVENUE, SUITE 210 SHOREWOOD, WI 53211 TEL: (414) 587-7459 CONTACT: DIMITRI DIMITROPOULOS

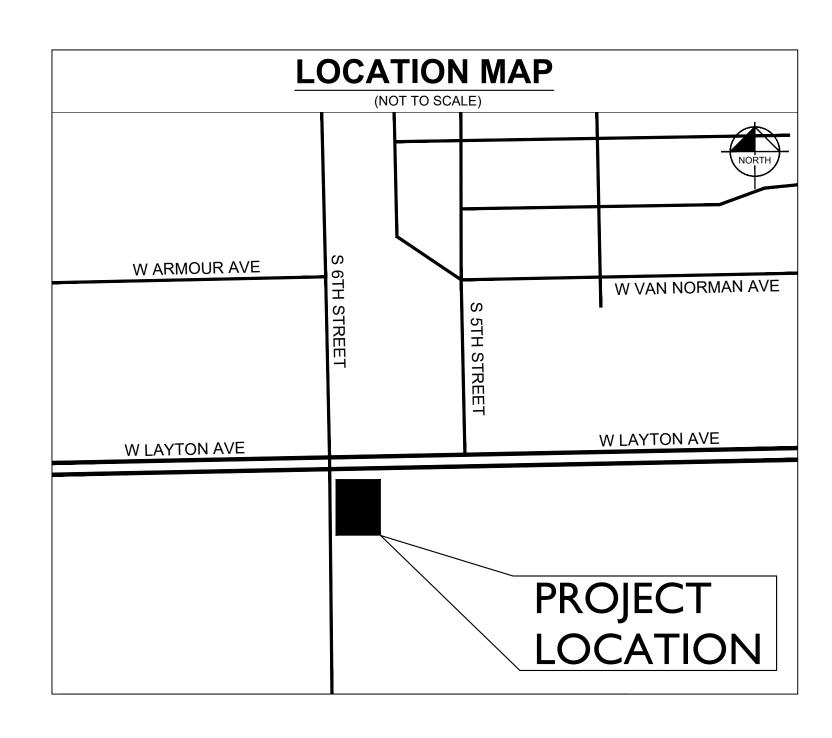
<u>SURVEYOR</u> CHAPUT 234 W. FLORIDA ST MILWAUKEE, WI 53204 TEL: (414) 224-8068 CONTACT: DONALD C. CHAPUT

<u>CIVIL ENGINEER</u> KIMLEY-HORN AND ASSOCIATES, INC. 111 W. JACKSON BLVD. CHICAGO, IL 60604 TEL: (630) 487-5550 EMAIL: JOÉ.MAYER@KIMLEY-HORN.COM CONTACT: JOE MAYER EMAIL: AMDY.HEINEN@KIMLEY-HORN.COM CONTACT: ANDY HEINEN, P.E.

LANDSCAPE ARCHITECT KIMLEY-HORN AND ASSOCIATES, INC. 4201 WINFIELD RD, SUITE 600 WARRENVILLE, IL 60555 TEL: (630) 487-5550 CONTACT: DANIEL GROVE, P.L.A.

# **LEGAL DESCRIPTION**

PARCEL 2 OF CERTIFIED SURVEY MAP NO. 6746, RECORDED NOVEMBER 1, 1999 AS DOCUMENT NO. 7837656, BEING A PART OF THE NORTHWEST 1/4 OF THE NORTHEAST 1/4 OF SECTION 29, TOWN 6 NORTH, RANGE 22 EAST, IN THE CITY OF MILWAUKEE, MILWAUKEE COUNTY, WISCONSIN.



#### **BENCHMARKS**

SITE BENCHMARKS:

(LOCATIONS SHOWN ON SURVEY) VERTICAL DATUM IS BASED ON NATIONAL GEODETIC VERTICAL DATUM OF 1929 CONVERTED TO CITY OF MILWAUKEE VERTICAL DATUM.

(NGVD29) - (580.603) = CITY OF MILWAUKEE VERTICAL DATUM

SBM #1 REFERENCE MARK AND REFERENCE BENCHMARK CHISELED CROSS IN TOP OF W SIDE OF HYD. ELEVATION=90.676

SBM #2 CHISELED CROSS S. SIDE OF CONC LP BASE ELEVATION=96.28

# PROFESSIONAL ENGINEER'S CERTIFICATION

I, ANDREW HEINEN, A LICENSED PROFESSIONAL ENGINEER OF WI, HEREBY CERTIFY THAT THIS SUBMISSION, PERTAINING ONLY TO THE "C" SERIES CIVIL SHEETS LISTED ABOVE BUT EXCLUDING DETAILS PREPARED BY OTHERS, WAS PREPARED ON BEHALF OF NEO GEN, INC. BY KIMLEY-HORN AND ASSOCIATES, INC. UNDER MY PERSONAL DIRECTION. THIS TECHNICAL SUBMISSION IS INTENDED TO BE USED AS AN INTEGRAL PART OF AND IN CONJUNCTION WITH THE PROJECT SPECIFICATIONS AND CONTRACT DOCUMENTS.

DATED THIS 8TH DAY OF FEBRUARY, A.D., 2023.

**Sheet List Table** 

GENERAL NOTES AND DETAILS

TOPOGRAPHIC SURVEY (BY OTHERS)

EXISTING CONDITIONS AND DEMO PLAN

GRADING AND EROSION CONTROL PLAN

COVER SHEET

SITE PLAN

Sheet Number | Sheet Title

C0.0

V0.0

C1.0

C3.0

A-NH: WI LICENSED PROFESSIONAL ENGINEER 36037-6 MY LICENSE EXPIRES ON NOVEMBER 30, 2023 DESIGN FIRM REGISTRATION NUMBER: 184002012-0006

234 W. FLORIDA STREET

MILWAUKEE, WI 53204

TEL: (414) 224-8068 COPIES OF THE SURVEY ARE AVAILABLE FROM THE ENGINEER. SITE CONDITIONS MAY HAVE CHANGED SINCE THE SURVEY WAS PREPARED. CONTRACTORS TO VISIT SITE TO FAMILIARIZE THEMSELVES WITH CURRENT CONDITIONS.

COPIES OF SOILS INVESTIGATION REPORTS MAY BE OBTAINED FROM THE OWNER. ANY BRACING, SHEETING OR SPECIAL CONSTRUCTION METHODS DEEMED NECESSARY BY THE CONTRACTOR IN ORDER TO INSTALL THE PROPOSED IMPROVEMENTS SHALL BE CONSIDERED INCIDENTAL TO THE COST OF THE PROJECT. ANY ADDITIONAL SOILS DATA NEEDED TO CONFIRM THE CONTRACTOR'S OPINIONS OF THE SUBSOIL CONDITIONS SHALL BE DONE AT THE CONTRACTOR'S EXPENSE. THE CONTRACTOR SHALL OBTAIN THE OWNER'S WRITTEN AUTHORIZATION TO ACCESS THE SITE TO CONDUCT A SUPPLEMENTAL SOILS INVESTIGATION.

THE CONTRACTOR SHALL PHOTOGRAPH THE WORK AREA PRIOR TO CONSTRUCTION FOR THE PURPOSE OF DOCUMENTING EXISTING CONDITIONS.

EXCEPT WHERE MODIFIED BY THE CONTRACT DOCUMENTS, ALL PROPOSED WORK SHALL BE IN ACCORDANCE WITH THE FOLLOWING SPECIFICATIONS WHICH ARE HEREBY MADE A PART HEREOF: A. "ROADWAY STANDARD SPECIFICATIONS, WISCONSIN DEPARTMENT OF TRANSPORTATION" AS PREPARED BY WISDOT, LATEST EDITION AND ITS SUPPLEMENTS.

B. 10 STATE RECOMMMENDED STANDARDS FOR WASTEWATER FACILITIES, AS PUBLISHED BY HEALTH RESEARCH, INC., LATEST EDITION.

C. ALL APPLICABLE PROVISIONS OF THE OCCUPATIONAL SAFETY AND HEALTH ACT ARE HEREIN INCORPORATED BY REFERENCE.

REGULATIONS, STANDARDS AND GENERAL REQUIREMENTS SET FORTH BY THE CITY OF MILWAUKEE, UNLESS OTHERWISE NOTED ON THE PLANS.

E. THE NATIONAL ELECTRIC CODE.

F. FEDERAL ADA STANDARDS FOR ACCESSIBLE DESIGN.

STANDARD SPECIFICATIONS, SUPPLEMENTAL SPECIFICATIONS, AND RECURRING SPECIAL PROVISIONS CONSTRUCTION PLANS, AND SUBSEQUENT DETAILS ARE ALL TO BE CONSIDERED AS PART OF THE CONTRACT. INCIDENTAL ITEMS OR ACCESSORIES NECESSARY TO COMPLETE THE CONTRACTOR'S WORK MAY NOT BE SPECIFICALLY NOTED, BUT ARE CONSIDERED A PART OF THE CONTRACTOR'S CONTRACT.

IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO ENSURE THAT ALL ITEMS REQUIRED FOR CONSTRUCTION OF THE PROJECT, AS SHOWN ON THE PLANS, ARE INCLUDED IN THE CONTRACT. ANY ITEM NOT SPECIFICALLY INCLUDED IN THE CONTRACT, BUT SHOWN ON THE PLANS, SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. THE CONTRACTOR SHALL NOTIFY THE ENGINEER IMMEDIATELY IN THE EVENT OF A DISCREPANCY WITH THE PLANS AND QUANTITIES.

THE CONTRACTOR IS RESPONSIBLE FOR HAVING A SET OF "APPROVED" ENGINEERING PLANS WITH THE LATEST REVISION DATE ON THE JOB SITE PRIOR TO THE START OF CONSTRUCTION. IF THERE ARE ANY DISCREPANCIES WITH WHAT IS SHOWN ON THE CONSTRUCTION PLANS, HE MUST IMMEDIATELY REPORT THEM TO THE SURVEYOR OR ENGINEER BEFORE DOING ANY WORK, OTHERWISE, THE CONTRACTOR ASSUMES FULL RESPONSIBILITY, IN THE EVENT OF DISAGREEMENT BETWEEN THE CONSTRUCTION PLANS. SPECIFICATIONS. AND/OR SPECIAL DETAILS. HE CONTRACTOR SHALL SECURE WRITTEN INSTRUCTION FROM THE ENGINEER PRIOR TO PROCEEDING WITH ANY PART OF THE WORK AFFECTED BY OMISSIONS OR DISCREPANCIES. FAILING TO SECURE SUCH INSTRUCTION, TH CONTRACTOR WILL BE CONSIDERED TO HAVE PROCEEDED AT THE CONTRACTOR'S OWN RISK AND EXPENSE. IN THE EVENT OF ANY DOUBT OR QUESTIONS ARISING WITH RESPECT TO THE TRUE MEANING OF THE CONSTRUCTION PLANS OR SPECIFICATIONS, THE DECISION OF THE ENGINEER SHALL BE FINAL AND CONCLUSIVE

THE CONTRACTOR SHALL SUBSCRIBE TO ALL GOVERNING REGULATIONS AND SHALL OBTAIN ALL NECESSARY PUBLIC AGENCY PERMITS PRIOR TO STARTING WORK. THE CONTRACTOR, BY USING THESE PLANS FOR THEIR WORK, AGREE TO HOLD HARMLESS KIMLEY—HORN AND ASSOCIATES, INC, THE CITY OF MILWAUKEE, THEIR EMPLOYEES AND AGENTS AND THE OWNER FROM AND AGAINST ANY AND ALL LIABILITY, CLAIMS, DAMAGES, AND THE COST OF DEFENSE ARISING OUT OF CONTRACTOR(S) PERFORMANCE OF THE WORK DESCRIBED HEREIN.

THE ENGINEER AND OWNER ARE NOT RESPONSIBLE FOR THE CONSTRUCTION MEANS, METHODS, TECHNIQUES, SEQUENCES OR PROCEDURES, TIME OF PERFORMANCE, PROGRAMS OR FOR ANY SAFETY PRECAUTIONS USED B' THE CONTRACTOR. THE CONTRACTOR IS SOLELY RESPONSIBLE FOR EXECUTION OF THE WORK IN ACCORDANCE THE CONTRACT DOCUMENTS AND SPECIFICATIONS.

D. CONSTRUCTION MATERIALS AND/OR EQUIPMENT MAY NOT BE STORED IN THE RIGHT-OF-WAY, AS DIRECTED BY THE OWNER.

. EASEMENTS FOR THE EXISTING UTILITIES, BOTH PUBLIC AND PRIVATE, AND UTILITIES WITHIN PUBLIC RIGHT-OF-WAYS ARE SHOWN ON THE PLANS ACCORDING TO AVAILABLE RECORDS. THE CONTRACTOR SHALL B RESPONSIBLE FOR DETERMINING THE EXACT LOCATION OF THESE UTILITY LINES AND THEIR PROTECTION FROM DAMAGE DUE TO CONSTRUCTION OPERATIONS. IF EXISTING UTILITY LINES OF ANY NATURE ARE ENCOUNTERED WHICH CONFLICT WITH LOCATIONS OF THE NEW CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE ENGINEER

OWNER SHALL OBTAIN EASEMENTS AND APPROVAL OF PERMITS NECESSARY TO FACILITATE CONSTRUCTION OF THE PROPOSED UTILITIES. THE CONTRACTOR, HOWEVER, SHALL FURNISH ALL REQUIRED BONDS AND EVIDENCE OF INSURANCE NECESSARY TO SECURE THESE PERMITS AND EASEMENTS.

. THE CONTRACTOR SHALL PRESERVE ALL CONSTRUCTION STAKES UNTIL THEY ARE NO LONGER NEEDED. ANY STAKES DESTROYED OR DISTURBED BY THE CONTRACTOR PRIOR TO THEIR USE SHALL BE RESET BY THE SURVEYOR AT THE CONTRACTOR'S EXPENSE.

. NOTIFICATION OF COMMENCING CONSTRUCTION:

14.A. THE CONTRACTOR SHALL NOTIFY AFFECTED GOVERNMENTAL AGENCIES IN WRITING AT LEAST THREE FULL WORKING DAYS PRIOR TO COMMENCEMENT OF CONSTRUCTION, IN ADDITION, THE CONTRACTOR SHALL NOTIFY, AS NECESSARY, ALL TESTING AGENCIES, THE CITY OF MILWAUKEE, AND THE OWNER SUFFICIENTLY IN ADVANCE OF CONSTRUCTION.

4.B. FAILURE OF THE CONTRACTOR TO ALLOW PROPER NOTIFICATION TIME WHICH RESULTS IN THE TESTING COMPANIES TO BE UNABLE TO VISIT THE SITE AND PERFORM TESTING WILL CAUSE THE CONTRACTOR TO SUSPEND THE OPERATION TO BE TESTED UNTIL THE TESTING AGENCY CAN SCHEDULE TESTING OPERATIONS

COST OF SUSPENSION OF WORK SHALL BE BORNE BY THE CONTRACTOR. 5. ALL CONTRACTORS SHALL KEEP ACCESS AVAILABLE AT ALL TIMES FOR ALL EMERGENCY TRAFFIC, AS DIRECTED

BY THE CITY OF MILWAUKEE 6. ANY EXISTING SIGNS, LIGHT STANDARDS, AND UTILITY POLES THAT INTERFERE WITH CONSTRUCTION OPERATIONS AND ARE NOT NOTED ON THE PLANS FOR DISPOSAL SHALL BE REMOVED AND RESET BY THE CONTRACTOR AT THE CONTRACTOR'S OWN EXPENSE, AS DIRECTED BY THE ENGINEER. ANY DAMAGE TO THESE ITEMS SHALL BE REPAIRED OR REPLACED BY THE CONTRACTOR AT THE CONTRACTOR'S OWN EXPENSE TO THE SATISFACTION OF

THE OWNER. ANY SIGNS NOT REQUIRED TO BE RESET SHALL BE DELIVERED TO THE RESPECTIVE OWNERS. ALL TREES TO BE SAVED SHALL BE IDENTIFIED PRIOR TO CONSTRUCTION BY THE LANDSCAPE ARCHITECT AND SHALL BE PROTECTED PER WISDOT SDD 14A1. THE RIGHT-OF-WAY LINE AND LIMITS OF THE CONTRACTOR'S OPERATIONS SHALL BE CLEARLY DEFINED THROUGHOUT THE CONSTRUCTION PERIOD. ALL TREES NOTED TO REMAIN SHALL BE PROTECTED FROM DAMAGE TO TRUNKS, BRANCHES AND ROOTS. NO EXCAVATING, FILLING OR

GRADING IS TO BE DONE INSIDE THE DRIP LINE OF TREES UNLESS OTHERWISE INDICATED. . LIMB PRUNING SHALL BE PERFORMED UNDER THE SUPERVISION OF AN APPROVED LANDSCAPE ARCHITECT, FORESTER. OR ARBORIST AND SHALL BE UNDERTAKEN IN A TIMELY FASHION SO AS NOT TO INTERFERE WITH CONSTRUCTION. ALL LIMBS, BRANCHES, AND OTHER DEBRIS RESULTING FROM THE CONTRACTOR'S WORK SHALL BE DISPOSED OF OFF-SITE BY THE CONTRACTOR AT THE CONTRACTOR'S OWN EXPENSE. ALL CUTS OVER ONE

1) INCH IN DIAMETER SHALL BE PAINTED WITH AN APPROVED TREE PAINT. 9. ALL EXISTING PAVEMENT OR CONCRETE TO BE REMOVED SHALL BE SAWCUT ALONG LIMITS OF PROPOSED REMOVAL BEFORE COMMENCEMENT OF PAVEMENT REMOVAL.

D. ALL EXISTING UTILITIES OR IMPROVEMENTS, INCLUDING WALKS, CURBS, PAVEMENT, AND PARKWAYS DAMAGED OR REMOVED DURING CONSTRUCTION SHALL BE PROMPTLY RESTORED TO THEIR RESPECTIVE ORIGINAL CONDITION. THE CONTRACTOR'S WORK SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT UNLESS A PAY ITEM IS LISTED ON THE BID LIST.

. REMOVAL OF SPECIFIED ITEMS, INCLUDING BUT NOT LIMITED TO, PAVEMENT, SIDEWALK, CURB, CURB AND GUTTER, CULVERTS, ETC., SHALL BE DISPOSED OF OFF-SITE BY THE CONTRACTOR AT THE CONTRACTOR'S OWN

EXPENSE. THE CONTRACTOR IS RESPONSIBLE FOR ANY PERMITS REQUIRED FOR SUCH DISPOSAL. . THE CONTRACTOR SHALL COLLECT AND REMOVE ALL CONSTRUCTION DEBRIS, EXCESS MATERIALS, TRASH, OIL AND GREASE RESIDUE, MACHINERY, TOOLS, AND OTHER MISCELLANEOUS ITEMS WHICH WERE NOT PRESENT PRIOR TO PROJECT COMMENCEMENT AT NO ADDITIONAL EXPENSE TO THE OWNER. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ACQUIRING ANY AND ALL PERMITS NECESSARY FOR THE HAULING AND DISPOSAL REQUIRED FOR CLEANUP, AS DIRECTED BY THE ENGINEER OR OWNER. BURNING ON THE SITE IS NOT PERMITTED.

3. NO UNDERGROUND WORK WITHIN THE PUBLIC RIGHT-OF-WAY SHALL BE COVERED UNTIL IT HAS BEEN APPROVED BY THE CITY OF MILWAUKEE. APPROVAL TO PROCEED MUST BE OBTAINED FROM THE CITY OF MILWAUKEE PRIOR TO INSTALLING PAVEMENT BASE, BINDER, AND SURFACE, AND PRIOR TO POURING ANY CONCRETE AFTER FORMS HAVE BEEN SET, AS NECESSARY.

. WHERE SHOWN ON THE PLANS OR DIRECTED BY THE ENGINEER, EXISTING DRAINAGE STRUCTURES AND PIPE SHALL BE CLEANED OF DEBRIS AND PATCHED AS NECESSARY TO ASSURE INTEGRITY OF THE STRUCTURE. THE CONTRACTOR'S WORK SHALL NOT BE PAID FOR SEPARATELY. BUT SHALL BE MERGED INTO THE CONTRACT UNIT PRICE EACH FOR STRUCTURES AND CONTRACT UNIT PRICE PER LINEAL FOOT FOR STORM SEWERS, WHICH SHALL BE PAYMENT IN FULL FOR CLEANING, PATCHING, REMOVAL, AND DISPOSAL OF DEBRIS AND DIR PRAINAGE STRUCTURES AND STORM SEWERS CONSTRUCTED AS PART OF THE CONTRACTOR'S PROJECT SHALL BE MAINTAINED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE. NO EXTRA PAYMENT WILL BE MADE FOR CLEANING STRUCTURES OR STORM SEWERS CONSTRUCTED AS PART OF THE CONTRACTOR'S PROJECT.

. THE CONTRACTOR SHALL BE RESPONSIBLE FOR HAVING THE UTILITY COMPANIES LOCATE THEIR FACILITIES IN THE FIELD PRIOR TO CONSTRUCTION AND SHALL ALSO BE RESPONSIBLE FOR THE MAINTENANCE AND PRESERVATION OF THESE FACILITIES. THE ENGINEER DOES NOT WARRANT THE LOCATION OF ANY EXISTING UTILITIES SHOWN ON THE PLANS. THE CONTRACTOR SHALL CALL DIGGERS HOTLINE LOCATING SERVICES (1-800-242-8511) AND THE CITY OF MILWAUKEE FOR UTILITY LOCATIONS.

THE GENERAL CONTRACTOR SHALL COORDINATE WITH UTILITY COMPANIES TO PROVIDE CABLE TV, PHONE, ELECTRIC, GAS AND IRRIGATION SERVICES. GENERAL CONTRACTOR SHALL BE RESPONSIBLE FOR SECURING SITE LAYOUTS FOR THESE UTILITIES AND SHALL COORDINATE AND PROVIDE CONDUIT CROSSINGS AS REQUIRED. THIS COORDINATION SHALL BE CONSIDERED INCIDENTAL TO GENERAL CONTRACTOR AGREEMENT WITH THE OWNER. ANY CONFLICTS IN UTILITIES SHALL BE CORRECTED BY THE GENERAL CONTRACTOR AT NO ADDITIONAL

27. CONTRACTOR IS TO VERIFY ALL EXISTING STRUCTURES AND FACILITIES AT ALL PROPOSED UTILITY CONNECTION LOCATIONS AND NOTIFY ENGINEER OF ANY DISCREPANCIES PRIOR TO ORDERING MATERIAL AND STARTING WORK. I. ANY FIELD TILES ENCOUNTERED SHALL BE INSPECTED BY THE ENGINEER. THE DRAIN TILE SHALL CONNECTED TO THE STORM SEWER SYSTEM AND A RECORD KEPT BY THE CONTRACTOR OF THE LOCATIONS AND TURNED OVER TO THE ENGINEER UPON COMPLETION OF THE PROJECT. THE COST OF THE CONTRACTOR'S WORK SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT, AND NO ADDITIONAL COMPENSATION SHALL BE

29. ALL FRAMES AND LIDS FOR STORM AND SANITARY SEWERS, VALVE VAULT COVERS, FIRE HYDRANTS, ANI B-BOXES ARE TO BE ADJUSTED TO MEET FINISHED GRADE. THE CONTRACTOR'S ADJUSTMENT IS TO BE MADE BY THE SEWER AND WATER CONTRACTOR, AND THE COST IS TO BE CONSIDERED INCIDENTAL. THESE ADJUSTMENTS TO FINISHED GRADE WILL NOT ALLEVIATE THE CONTRACTOR FROM ANY ADDITIONAL ADJUSTMENTS AS REQUIRED BY THE CITY OF MILWAUKEE UPON FINAL INSPECTION OF THE PROJECT

D. HYDRANTS SHALL NOT BE FLUSHED DIRECTLY ONTO THE ROAD SUBGRADES. WHENEVER POSSIBLE, HOSES SHALL BE USED TO DIRECT THE WATER INTO LOT AREAS OR THE STORM SEWER SYSTEM, IF AVAILABLE. DAMAGE TO THE ROAD SUBGRADE OR LOT GRADING DUE TO EXCESSIVE WATER SATURATION AND/OR EROSION FROM HYDRANT FLUSHING, OR FROM LEAKS IN THE WATER DISTRIBUTION SYSTEM, WILL BE REPAIRED BY THE CONTRACTOR FLUSHING OR USING THE HYDRANT AT THE CONTRACTOR'S OWN EXPENSE. LEAKS IN THE WATER DISTRIBUTION SYSTEM SHALL BE THE RESPONSIBILITY OF THE WATER MAIN CONTRACTOR AND SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE. 31. TRENCH BACKFILL WILL BE REQUIRED TO THE FULL DEPTH ABOVE SEWERS AND WATERMAIN WITHIN TWO (2) FEET HORIZONTAL OF PROPOSED OR EXISTING PAVEMENT

32. IF SOFT, SPONGY, OR OTHER UNSUITABLE SOILS WITH UNCONFINED COMPRESSIVE STRENGTH LESS THAN 0.5 TSF ARE ENCOUNTERED AT THE BOTTOM OF THE TRENCH, ALL SUCH MATERIAL SHALL BE REMOVED AND REPLACED WITH WELL-COMPACTED, CRUSHED LIMESTONE BEDDING MATERIAL. IF ROCK IS ENCOUNTERED, IT SHALL BE REMOVED TO AT LEAST SIX (6) INCHES BELOW THE BOTTOM OF THE PIPE TO ALLOW PROPER THICKNESS OF BEDDING. ANY UNDERCUTS OF TWO (2) FEET OR LESS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. DEPTHS GREATER THAN TWO (2) FEET SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL

33. THE TRENCHES FOR PIPE INSTALLATION SHALL BE KEPT DRY AT ALL TIMES DURING PIPE PLACEMENT. APPROPRIATE FACILITIES TO MAINTAIN THE DRY TRENCH SHALL BE PROVIDED BY THE CONTRACTOR, AND THE COST OF SUCH SHALL BE INCIDENTAL TO THE UNIT PRICE BID FOR THE ITEM. PLANS FOR THE SITE DEWATERING, IF EMPLOYED, SHALL BE SUBMITTED TO AND APPROVED BY THE OWNER PRIOR TO IMPLEMENTATION. NO ADDITIONAL COMPENSATION SHALL BE MADE FOR DEWATERING DURING CONSTRUCTION

UNLESS APPROVED IN WRITING BY THE OWNER. 34. AFTER THE STORM SEWER SYSTEM HAS BEEN CONSTRUCTED, THE CONTRACTOR SHALL PLACE PROPER INLET PROTECTION EROSION CONTROL AT LOCATIONS INDICATED BY THE ENGINEER. THE PURPOSE OF THE INLET PROTECTION WILL BE TO MINIMIZE THE AMOUNT OF SILTATION THAT NORMALLY WOULD ENTER THE STORM

SEWER SYSTEM FROM ADJACENT AND/OR UPSTREAM DRAINAGE AREAS. 35. AT THE CLOSE OF EACH WORKING DAY AND AT THE CONCLUSION OF CONSTRUCTION OPERATIONS, ALL

36. EROSION CONTROL MEASURES SHALL BE INSTALLED IN ACCORDANCE WITH THE WISCONSIN DEPARTMENT OF NATURAL RESOURCES REGULATIONS AND WISDOT STANDARDS FOR SOIL EROSION AND SEDIMENTATION CONTROL AND SHALL BE MAINTAINED BY THE CONTRACTOR AND REMAIN IN PLACE UNTIL A SUITABLE GROWTH OF GRASS, ACCEPTABLE TO THE ENGINEER, HAS DEVELOPED.

DRAINAGE STRUCTURES AND FLOW LINES SHALL BE FREE FROM DIRT AND DEBRIS.

37. THE CONTRACTOR SHALL CONFORM TO ALL EROSION CONTROL REQUIREMENTS AS SET FORTH BY THE WISCONSIN DEPARTMENT OF NATURAL RESOURCES THROUGH THE NPDES PHASE II PERMIT PROGRAM REQUIREMENTS AND GOVERNING MUNICIPALITY. THE CONTRACTOR SHALL INSTALL AND MAINTAIN ALL EROSION CONTROL MEASURES AS INDICATED ON THE EROSION CONTROL DRAWINGS AND SPECIFICATIONS PREPARED BY KIMLEY-HORN AND ASSOCIATES, INC. KIMLEY-HORN AND ASSOCIATES, INC. IS NOT RESPONSIBLE FOR THE ACTS OR OMISSIONS OF THE CONTRACTOR, SUBCONTRACTORS OR SUPPLIERS, WHICH CONTRIBUTE T DEFICIENCIES IN THE EROSION CONTROL PLAN OR ANY VIOLATIONS RESULTING FROM INADEQUATE EROSION CONTROL PROTECTION AND/OR DOCUMENTATION.

38. THE PAVEMENT SHALL BE KEPT FREE OF MUD AND DEBRIS AT ALL TIMES. IT MAY BE NECESSARY TO KEEP A SWEEPER ON-SITE AT ALL TIMES.

39. ALL DISTURBED AREAS OF THE RIGHT-OF-WAY SHALL BE FULLY RESTORED TO PRE-CONSTRUCTION CONDITIONS WITH A MINIMUM OF SIX (6) INCHES OF TOPSOIL, SEEDING, AND MULCH AS PER WISDOT

41. ALL TESTING SHALL BE THE RESPONSIBILITY AND EXPENSE OF THE CONTRACTOR. IF REQUESTED BY

40. ALL PROPOSED GRADES SHOWN ON PLANS ARE FINISHED SURFACE ELEVATIONS, UNLESS NOTED OTHERWISE.

THE CITY OF MILWAUKEE OR ENGINEER, COPIES OF ALL TEST RESULTS SHALL BE PROVIDED TO THE ENGINEER

42. PROVIDE SMOOTH VERTICAL CURVES THROUGH HIGH AND LOW POINTS INDICATED BY SPOT ELEVATIONS. PROVIDE UNIFORM SLOPES BETWEEN NEW AND EXISTING GRADES. AVOID RIDGES AND DEPRESSIONS.

3. WHEN REQUIRED, THE CONTRACTOR SHALL NOTIFY THE OWNER WHEN RECORD DRAWINGS CAN BE PREPARED. RECORD DRAWINGS SHALL INDICATE THE FINAL LOCATION AND LAYOUT OF ALL IMPROVEMENTS, INCLUDING VERIFICATION OF ALL CONCRETE PADS, INVERT, RIM, AND SPOT GRADE ELEVATIONS, AND INCORPORATE ALL FIELD DESIGN CHANGES APPROVED BY THE OWNER.

44. BEFORE ACCEPTANCE, ALL WORK SHALL BE INSPECTED BY THE CITY OF MILWAUKEE, AS NECESSARY.

#### EARTHWORK NOTES

1.1. IT IS THE CONTRACTOR'S RESPONSIBILITY TO UNDERSTAND THE SOIL AND GROUNDWATER CONDITIONS AT

1.2. ANY QUANTITIES IN THE BID PROPOSAL ARE INTENDED AS A GUIDE FOR THE CONTRACTOR'S USE IN DETERMINING THE SCOPE OF THE COMPLETED PROJECT. IT IS THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE ALL MATERIAL QUANTITIES AND BE KNOWLEDGEABLE OF ALL SITE CONDITIONS.

1.3. THE CONTRACTOR WILL NOTE THAT THE ELEVATIONS SHOWN ON THE CONSTRUCTION PLANS ARE FINISHED GRADE AND THAT PAVEMENT THICKNESS, TOPSOIL, ETC., MUST BE ACCOUNTED FOR.

1.4. THE CONTRACTOR SHALL MAINTAIN POSITIVE DRAINAGE DURING CONSTRUCTION AND PREVENT STORMWATER FROM RUNNING INTO OR STANDING IN EXCAVATED AREAS. THE FAILURE TO PROVIDE PROPER DRAINAGE WILL NEGATE ANY POSSIBLE ADDED COMPENSATION REQUESTED DUE TO DELAYS OR UNSUITABLE MATERIALS CREATED AS A RESULT THEREOF. FINAL GRADES SHALL BE PROTECTED AGAINST DAMAGE FROM EROSION, SEDIMENTATION, AND TRAFFIC.

.5. THE CONTRACTOR SHALL BE RESPONSIBLE FOR IMPLEMENTATION OF THE SOIL EROSION AND SEDIMENTATION CONTROL MEASURES. THE INITIAL ESTABLISHMENT OF EROSION CONTROL PROCEDURES AND THE PLACEMENT OF SILT AND FILTER FENCING, ETC., TO PROTECT ADJACENT PROPERTY, WETLANDS, ETC., SHALL OCCUR BEFORE GRADING BEGINS.

.6. PRIOR TO COMMENCEMENT OF GRADING ACTIVITIES, THE CONTRACTOR SHALL ERECT A CONSTRUCTION FENCE AROUND ANY TREE DESIGNATED TO BE PRESERVED. SAID FENCE SHALL BE PLACED IN A CIRCLE CENTERED AROUND THE TREE. THE DIAMETER OF WHICH SHALL BE SUCH THAT THE ENTIRE DRIP ZONE (EXTENT OF FURTHEST EXTENDING BRANCHES) SHALL BE WITHIN THE FENCE LIMITS. THE EXISTING GRADE WITHIN THE FENCED AREA SHALL NOT BE DISTURBED. TOPSOIL EXCAVATION INCLUDES

2.1. EXCAVATION OF TOPSOIL AND OTHER STRUCTURALLY UNSUITABLE MATERIALS WITHIN THOSE AREAS THAT WILL REQUIRE EARTH EXCAVATION OR COMPACTED EARTH FILL MATERIAL. EXISTING VEGETATION SHALL BE REMOVED PRIOR TO STRIPPING TOPSOIL OR FILLING AREAS.

2.2. PLACEMENT OF EXCAVATED MATERIAL IN OWNER-DESIGNATED AREAS FOR FUTURE USE WITHIN AREAS TO BE LANDSCAPED AND THOSE AREAS NOT REQUIRING STRUCTURAL FILL MATERIAL. PROVIDE NECESSARY EROSION CONTROL MEASURES FOR STOCKPILE.

2.3. TOPSOIL STOCKPILED FOR RESPREAD SHALL BE FREE OF CLAY AND SHALL NOT CONTAIN ANY OF THE TRANSITIONAL MATERIAL BETWEEN THE TOPSOIL AND CLAY. THE TRANSITIONAL MATERIAL SHALL BE USED IN NON-STRUCTURAL FILL AREAS OR DISPOSED OF OFF-SITE. 2.4. TOPSOIL RESPREAD SHALL INCLUDE HAULING AND SPREADING SIX (6) INCHES OF TOPSOIL DIRECTLY

OVER AREAS TO BE LANDSCAPED WHERE SHOWN ON THE PLANS OR AS DIRECTED BY THE OWNER. 2.5. MODERATE COMPACTION IS REQUIRED IN NON-STRUCTURAL FILL AREAS.

3.1. EXCAVATION OF SUBSURFACE MATERIALS WHICH ARE SUITABLE FOR USE AS STRUCTURAL FILL. THE EXCAVATION SHALL BE TO WITHIN A TOLERANCE OF 0.1 FEET OF THE PLAN SUBGRADE ELEVATIONS WHILE MAINTAINING PROPER DRAINAGE. THE TOLERANCE WITHIN PAVEMENT AREAS SHALL BE SUCH THAT THE EARTH MATERIALS SHALL "BALANCE" DURING THE FINE GRADING OPERATION.

.2. PLACEMENT OF SUITABLE MATERIALS SHALL BE WITHIN THOSE AREAS REQUIRING STRUCTURAL FILL IN ORDER TO ACHIEVE THE PLAN SUBGRADE ELEVATIONS TO WITHIN A TOLERANCE OF 0.1 FEET. THE FILL MATERIALS SHALL BE PLACED IN LOOSE LIFTS THAT SHALL NOT EXCEED EIGHT (8) INCHES IN THICKNESS, AND THE WATER CONTENT SHALL BE ADJUSTED IN ORDER TO ACHIEVÉ REQUIRED

3.3. STRUCTURAL FILL MATERIAL MAY BE PLACED WITHIN THOSE PORTIONS OF THE SITE NOT REQUIRING STRUCTURAL FILL, WITHIN SIX (6) INCHES OF THE PLAN FINISHED GRADE ELEVATION. IN AREAS REQUIRING STRUCTURAL FILL, HOWEVER, THIS MATERIAL SHALL NOT BE PLACED OVER TOPSOIL OR OTHER UNSUITABLE MATERIALS UNLESS SPECIFICALLY DIRECTED BY A SOILS ENGINEER WITH THE CONCURRENCE 3.4. COMPACTION OF SUITABLE MATERIALS SHALL BE TO AT LEAST 93% OF THE MODIFIED PROCTOR DRY DENSITY WITHIN PROPOSED PAVEMENT AREAS, SIDEWALK, ETC. COMPACTION SHALL BE AT LEAST 95% OF

THE MODIFIED PROCTOR WITHIN PROPOSED BUILDING PAD AREAS. UNSUITABLE MATERIAL: UNSUITABLE MATERIALS SHALL BE CONSIDERED MATERIAL THAT IS NOT SUITABLE FOR THE SUPPORT OF PAVEMENT AND BUILDING CONSTRUCTION, AND IS ENCOUNTERED BELOW NORMAL TOPSOIL DEPTHS AND THE PROPOSED SUBGRADE ELEVATION. THE DECISION TO REMOVE SAID MATERIAL

AND TO WHAT EXTENT SHALL BE MADE BY THE ENGINEER WITH THE CONCURRENCE OF THE OWNER. MISCELLANEOUS. THE CONTRACTOR SHALL:

DEGREES FAHRENHEIT AND RISING.

5.1. SPREAD AND COMPACT UNIFORMLY TO THE DEGREE SPECIFIED ALL EXCESS TRENCH SPOIL AFTER COMPLETION OF THE UNDERGROUND IMPROVEMENTS.

5.2. SCARIFY, DISC, AERATE, AND COMPACT, TO THE DEGREE SPECIFIED, THE UPPER TWELVE (12) INCHES OF THE SUITABLE SUBGRADE MATERIAL IN ALL AREAS THAT MAY BE SOFT DUE TO EXCESS MOISTURE CONTENT. THIS APPLIES TO CUT AREAS AS WELL AS FILL AREAS.

PURPOSE OF ACHIEVING THE SPECIFIED COMPACTION. 5.4. BACKFILL THE CURB AND GUTTER AFTER ITS CONSTRUCTION AND PRIOR TO THE PLACEMENT OF THE BASE COURSE MATERIAL.

5.3. PROVIDE WATER TO ADD TO DRY MATERIAL IN ORDER TO ADJUST THE MOISTURE CONTENT FOR THE

6.1. THE CONTRACTOR SHALL PROVIDE AS A MINIMUM A FULLY LOADED SIX-WHEEL TANDEM AXLE TRUCK FOR PROOF ROLLING THE PAVEMENT SUBGRADE PRIOR TO THE PLACEMENT OF THE CURB AND GUTTER AND THE BASE MATERIAL. THIS SHALL BE WITNESSED BY THE ENGINEER AND THE OWNER. (SEE PAVING

6.2. ANY UNSUITABLE AREA ENCOUNTERED AS A RESULT OF PROOF ROLLING SHALL BE REMOVED AND REPLACED WITH SUITABLE MATERIAL OR OTHERWISE CORRECTED AND APPROVED BY THE ENGINEER.

#### SIGNAGE AND PAVEMENT MARKING NOTES

ALL SIGNING AND PAVEMENT MARKING SHALL BE IN ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) AND THE WISCONSIN DEPARTMENT OF TRANSPORTATION (WISDOT) STANDARDS. 2. SIGNS: SIGNS SHALL BE CONSTRUCTED OF 0.080—INCH THICK FLAT ALUMINUM PANELS WITH REFLECTORIZED LEGEND ON THE FACE. LEGEND SHALL BE IN ACCORDANCE WITH THE MUTCD.

. POSTS: SIGN POSTS SHALL BE A HEAVY—DUTY STEEL "U" SHAPED CHANNEL WEIGHING 3.0 POUNDS/FOOT SUCH AS A TYPE B METAL POST, AS PER THE IDOT STANDARDS (OR 2—INCH PERFORATED STEEL TUBE). 4. SIGNS AND POSTS SHALL BE INSTALLED IN ACCORDANCE WITH WISDOT STANDARDS

5. PAVEMENT MARKINGS: ALL PAVEMENT MARKINGS IN THE PUBLIC RIGHT-OF-WAY, SUCH AS STOP LINES,

CENTERLINES, CROSSWALKS, AND DIRECTIONAL ARROWS, SHALL BE REFLECTORIZED THERMOPLASTIC.

6. PAVEMENT MARKINGS ON BIKE PATHS, PARKING LOT STALLS, AND SIMILAR "LOW-WEAR" APPLICATIONS, 7. COLOR, WIDTH, STYLE, AND SIZE OF ALL MARKINGS SHALL BE IN ACCORDANCE WITH THE MUTCD AND LOCAL CODE. STANDARD PARKING SPACES SHALL BE PAINTED WHITE OR YELLOW PER LOCAL CODE.

8. THERMOPLASTIC MARKINGS SHALL BE INSTALLED WHEN THE PAVEMENT TEMPERATURE IS 55 DEGREES FAHRENHEIT AND RISING. PAINT MARKINGS MAY BE INSTALLED WHEN THE AIR TEMPERATURE IS 50

**PAVING NOTES** 

1.1. PAVING WORK INCLUDES FINAL SUBGRADE SHAPING, PREPARATION, AND COMPACTION; PLACEMENT OF SUBBASE OR BASE COURSE MATERIALS; BITUMINOUS BINDER AND/OR SURFACE COURSES; FORMING,

1/4" WIDE SAWCUT, 1" DEEP,

.C. CONCRETE PAVEMENT -

15' O.C. MAX

#4 EPOXY COATED -

V . 74

─ AGGREGATE BASE COURSE,

COMPACTED TO 95%

DOWELS @ 18" O.C

JOINT

CONSTRUCTION -

- COMPACTED SUB-GRADE OR TRENCH

BACKFILL TO AT LEAST 95% OF THE

FINISHING, AND CURING CONCRETE PAVEMENT, CURBS, AND WALKS; AND FINAL CLEAN-UP AND ALL 1.2. COMPACTION REQUIREMENTS [REFERENCE ASTM D-1557 (MODIFIED PROCTOR)]: SUBGRADE = 93%;

SUBBASE = 93%; AGGREGATE BASE COURSE = 95%; BITUMINOUS COURSES = 95% OF MAXIMUM DENSITY, PER WISCONSIN DEPARTMENT OF TRANSPORTATION (WISDOT) HIGHWAY STANDARDS. 1.3. IT SHALL BE THE CONTRACTOR'S SOLE RESPONSIBILITY TO PROVIDE PROPER BARRICADING WARNING DEVICES, AND THE SAFE MANAGEMENT OF TRAFFIC WITHIN THE AREA OF CONSTRUCTION. ALL SUCH

DEVICES (MUTCD), LATEST EDITION, AND IN ACCORDANCE WITH THE CITY OF MILWAUKEE CODE.

2.1. EARTHWORK FOR PROPOSED PAVEMENT SUBGRADE SHALL BE FINISHED TO WITHIN 0.1 FOOT, PLUS OR MINUS, OF PLAN ELEVATION. THE CONTRACTOR SHALL CONFIRM THAT THE SUBGRADE HAS BEEN PROPERLY PREPARED AND THAT THE FINISHED TOP SUBGRADE ELEVATION HAS BEEN GRADED WITHIN TOLERANCES ALLOWED IN THESE SPECIFICATIONS, UNLESS THE CONTRACTOR ADVISES THE ENGINEER IN WRITING PRIOR TO FINE GRADING FOR BASE COURSE CONSTRUCTION. IT IS UNDERSTOOD THAT THE CONTRACTOR HAS APPROVED AND ACCEPTS THE RESPONSIBILITY FOR THE SUBGRADE.

DEVICES AND THEIR INSTALLATION SHALL CONFORM TO THE MANUAL OF UNIFORM TRAFFIC CONTROL

2. PRIOR TO THE PLACEMENT OF THE BASE COURSE, THE SUBGRADE MUST BE PROOF—ROLLED AND INSPECTED FOR UNSUITABLE MATERIALS AND/OR EXCESSIVE MOVEMENT. IF UNSUITABLE SUBGRADE IS ENCOUNTERED, IT SHALL BE CORRECTED. THIS MAY INCLUDE ONE OR MORE OF THE FOLLOWING

2.2.1. SCARIFY, DISC, AND AERATE.

2.2.2. REMOVE AND REPLACE WITH STRUCTURAL CLAY FILL.

2.2.3. REMOVE AND REPLACE WITH GRANULAR MATERIAL

2.2.4. USE OF GEOTEXTILE FABRIC.

2.2.5. MAXIMUM DEFLECTION ALLOWED IN ISOLATED AREAS MAY BE ONE-QUARTER (1/4) INCH TO ONE-HALF (1/2) INCH IF NO DEFLECTION OCCURS OVER THE MAJORITY OF THE AREA.

2.3. PRIOR TO THE CONSTRUCTION OF THE CURB AND GUTTER AND THE PLACEMENT OF THE BASE MATERIAL, THE PAVEMENT AREA SHALL BE FINE—GRADED TO WITHIN 0.04 FEET (1/2 INCH) OF FINAL SUBGRADE ELEVATION, TO A POINT TWO (2) FEET BEYOND THE BACK OF THE CURB, SO AS TO ENSURE THE PROPER THICKNESS OF PAVEMENT COURSES. NO CLAIMS FOR EXCESS QUANTITY OF BASE MATERIALS DUE TO IMPROPER SUBGRADE PREPARATION WILL BE HONORED.

2.4. PRIOR TO PLACEMENT OF THE BASE COURSE, THE SUBGRADE SHALL BE APPROVED BY THE TESTING ENGINEER.

3.1. ALL EXTERIOR CONCRETE SHALL BE PORTLAND CEMENT CONCRETE WITH AIR ENTRAINMENT OF NO LESS THAN FIVE (5%) OR MORE THAN EIGHT (8%) PERCENT. CONCRETE SHALL BE A MINIMUM OF SIX ) BAG MIX AND SHALL DEVELOP A MINIMUM OF 3,500 PSI COMPRESSIVE STRENGTH AT FOURTEEN 14) DAYS AND A MINIMUM OF 4,000 PSI COMPRESSIVE STRENGTH AT TWENTY—EIGHT (28) DAYS. ALL CONCRETE SHALL BE BROOM-FINISHED PERPENDICULAR TO THE DIRECTION OF TRAVEL.

3.2. CONCRETE CURB AND/OR COMBINATION CURB AND GUTTER SHALL BE OF THE TYPE SHOWN ON THE PLANS. THE CONTRACTOR IS CAUTIONED TO REFER TO THE CONSTRUCTION STANDARDS AND THE PAVEMENT CROSS SECTION TO DETERMINE THE GUTTER FLAG THICKNESS AND THE AGGREGATE BASE COURSE THICKNESS BENEATH THE CURB AND GUTTER. PRE-MOLDED FIBER EXPANSION JOINTS, WITH WO 3/4-INCH BY 18-INCH EPOXY-COATED STEEL DOWEL BARS, SHALL BE GREASED AND FITTED WITH METAL EXPANSION TUBES

3.3. CURBS SHALL BE DEPRESSED AND MEET THE SLOPE REQUIREMENTS OF THE FEDERAL ADA STANDARDS FOR ACCESSIBLE DESIGN AT LOCATIONS WHERE PUBLIC WALKS INTERSECT CURB LINES AND OTHER LOCATIONS, AS DIRECTED, FOR THE PURPOSE OF PROVIDING ACCESSIBILITY.

3.4. THE CURBS SHALL BE BACKFILLED AFTER THEIR CONSTRUCTION AND PRIOR TO THE PLACEMENT OF THE BASE COURSE.

3.5. CONCRETE SIDEWALK SHALL BE IN ACCORDANCE WITH THE ABOVE AND THE PLANS. PROVIDE SCORED JOINTS AT 5-FOOT INTERVALS AND 1/2-INCH PRE-MOLDED FIBER EXPANSION JOINTS AT 20-FOOT INTERVALS AND ADJACENT TO CONCRETE CURBS, DRIVEWAYS, FOUNDATIONS, AND OTHER

3.6. CONCRETE CURING AND PROTECTION SHALL BE PER WISDOT STANDARDS. TWO (2) COATS OF WISDOT APPROVED CURING AGENT SHALL BE APPLIED TO ALL EXPOSED CONCRETE SURFÁCES. 3.7. THE COST OF AGGREGATE BASE OR SUBBASE UNDER CONCRETE WORK SHALL BE INCLUDED IN THE COST OF THE RESPECTIVE CONCRETE ITEM.

4.1. THE PAVEMENT MATERIALS FOR BITUMINOUS STREETS, PARKING LOTS, AND DRIVE AISLES SHALL BE AS DETAILED ON THE PLANS. UNLESS OTHERWISE SHOWN ON THE PLANS, THE FLEXIBLE PAVEMENTS SHALL CONSIST OF AGGREGATE BASE COURSE, BITUMINOUS CONCRETE BINDER COURSE; AND BITUMINOUS CONCRETE SURFACE COURSE, OF THE THICKNESS AND MATERIALS SPECIFIED ON THE PLANS. THICKNESSES SPECIFIED SHALL BE CONSIDERED TO BE THE MINIMUM COMPACTED THICKNESS 4.2. ALL TRAFFIC SHALL BE KEPT OFF THE COMPLETED AGGREGATE BASE UNTIL THE BINDER COURSE IS

LAID. THE AGGREGATE BASE SHALL BE UNIFORMLY PRIME COATED AT A RATE OF 0.4 TO 0.5 GALLONS PER SQUARE YARD PRIOR TO PLACING THE BINDER COURSE. PRIME COAT MATERIALS SHALL BE WISDOT APPROVED. 4.3. PRIOR TO PLACEMENT OF THE SURFACE COURSE, THE BINDER COURSE SHALL BE CLEANED AND

TACK-COATED IF DUSTY OR DIRTY. ALL DAMAGED AREAS IN THE BINDER, BASE, OR CURB SHALL BE REPAIRED TO THE SATISFACTION OF THE OWNER PRIOR TO LAYING THE SURFACE COURSE. T CONTRACTOR SHALL PROVIDE WHATEVER EQUIPMENT AND STAFF NECESSARY, INCLUDING THE USE POWER BROOMS IF REQUIRED BY THE OWNER, TO PREPARE THE PAVEMENT FOR APPLICATION OF THE SURFACE COURSE. THE TACK COAT SHALL BE UNIFORMLY APPLIED TO THE BINDER COURSE AT A RATE OF 0.05 TO 0.10 GALLONS PER SQUARE YARD. TACK COAT SHALL BE AS PER WISDOT

4.4. SEAMS IN BAM, BINDER, AND SURFACE COURSE SHALL BE STAGGERED A MINIMUM OF 6 INCHES. 5. TESTING AND FINAL ACCEPTANCE.

5.1. THE CONTRACTOR SHALL FOLLOW THE QUALITY CONTROL TESTING PROGRAM FOR CONCRETE AND PAVEMENT MATERIALS ESTABLISHED BY THE ENGINEER.

5.2. PRIOR TO PLACEMENT OF THE BITUMINOUS CONCRETE SURFACE COURSE, THE CONTRACTOR, WHEN

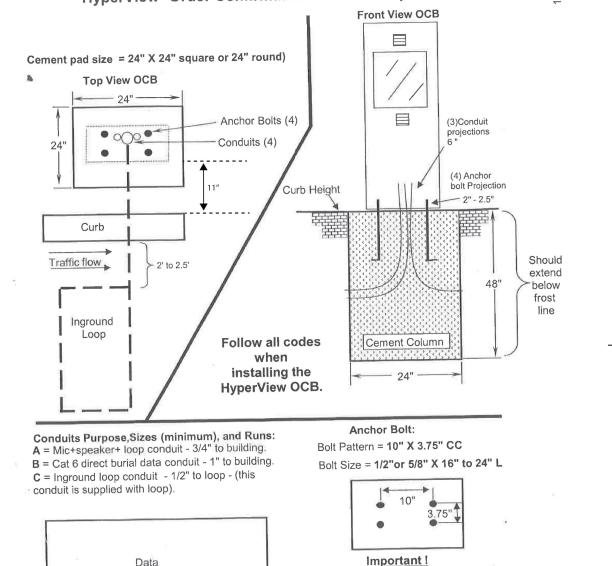
REQUIRED BY THE CITY OF MILWAUKEE, SHALL OBTAIN SPECIMENS OF THE BINDER COURSE WITH A CORE DRILL WHERE DIRECTED, FOR THE PURPOSE OF THICKNESS VERIFICATION. 5.3. WHEN REQUIRED BY THE CITY OF MILWAUKEE, THE CONTRACTOR SHALL OBTAIN SPECIMENS OF THE FULL DEPTH BITUMINOUS CONCRETE PAVEMENT STRUCTURE WITH A CORE DRILL WHERE DIRECTED IN ORDER TO CONFIRM THE PLAN THICKNESS. DEFICIENCIES IN THICKNESS SHALL BE ADJUSTED FOR BY

HE METHOD REQUIRED BY WISDOT STANDARDS. 5.4. FINAL ACCEPTANCE OF THE TOTAL PAVEMENT INSTALLATION SHALL BE SUBJECT TO THE TESTING AND CHECKING REQUIREMENTS CITED ABOVE.

ALL MATERIAL AND CONSTRUCTION SHALL CONFORM TO THE CITY OF MILWAUKEE CODE. WHEN

CONFLICTS ARISE BETWEEN MUNICIPAL CODE, GENERAL NOTES AND SPECIFICATIONS, THE MORE

STRINGENT SHALL TAKE PRECEDENCE. STANDARD DUTY **ASPHALT PAVEMENT SECTION** HyperView- Order Confirmation Board - Requirements



Keep conduits and anchor bolts centered

Be sure to place the menu

board in a position that will not

be obstructed by the Order

Confirmation Board!

on cement pad

FINISH GRADE/PAVEMENT - CONCRETE FOOTING

- SUBGRADE-SCARIFIED AND COMPACTED

ROUNDED CONCRETE TOF

TO AT LEAST 95% OF THE MODIFIED

PROCTOR MAXIMUM DRY DENSITY

PAVEMENT THICKNESS BASED ON GENERAL

WHICHEVER IS MORE STRINGENT.

UNDERSTANDING OF THE AREA. NO GEOTECHNICAL

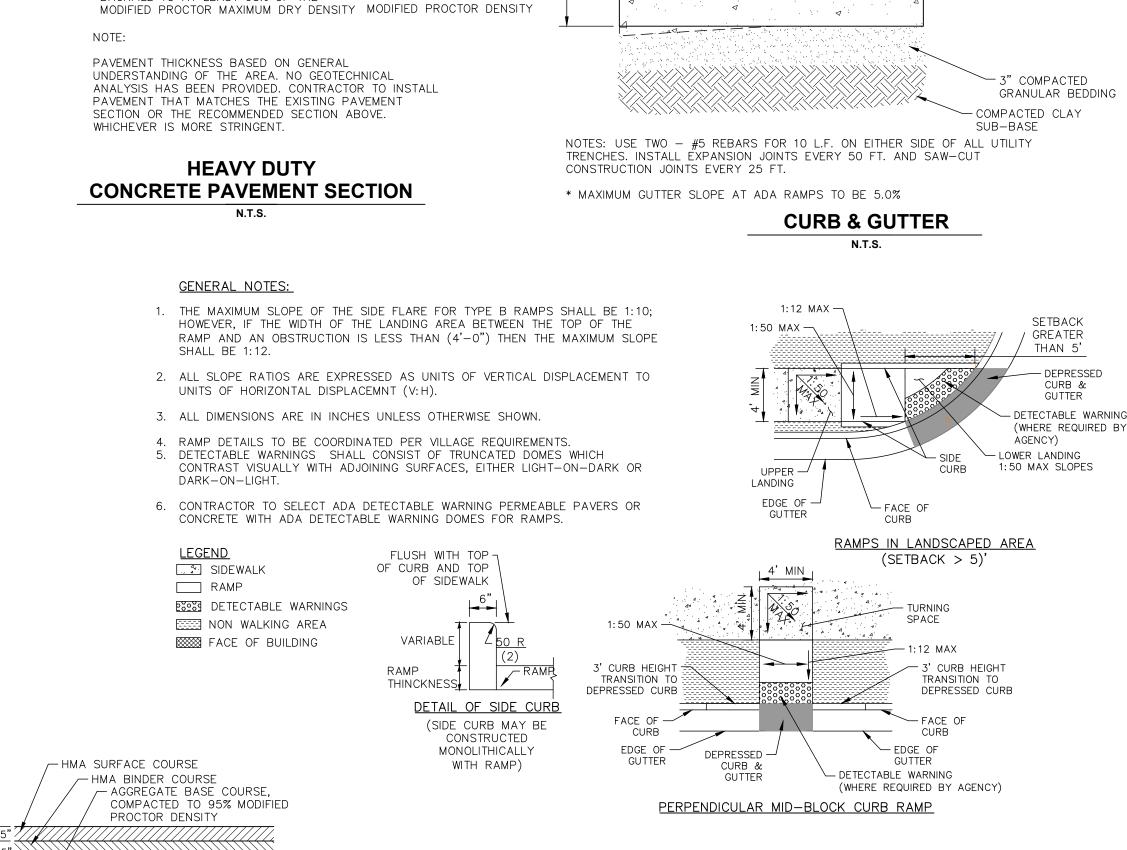
SECTION OR THE RECOMMENDED SECTION ABOVE.

PAVEMENT THAT MATCHES THE EXISTING PAVEMENT

ANALYSIS HAS BEEN PROVIDED. CONTRACTOR TO INSTALL

**5" BOLLARD DETAIL** 

DIAMETER



REVERSE GUTTER PITCH -

3/4" PER FT.\*

SIZE VARIES, SEE

SHEET C4.0 FOR

DETAILS

·2" FOR DEPRESSED CURB AT

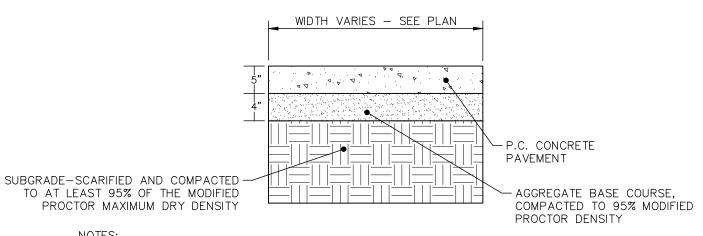
DRIVE ENTRANCES

DEPRESSED CURB AT ADA RAMPS

1/2" MAX. FOR SPECIAL

- 3500 P.S.I. CONCRETE



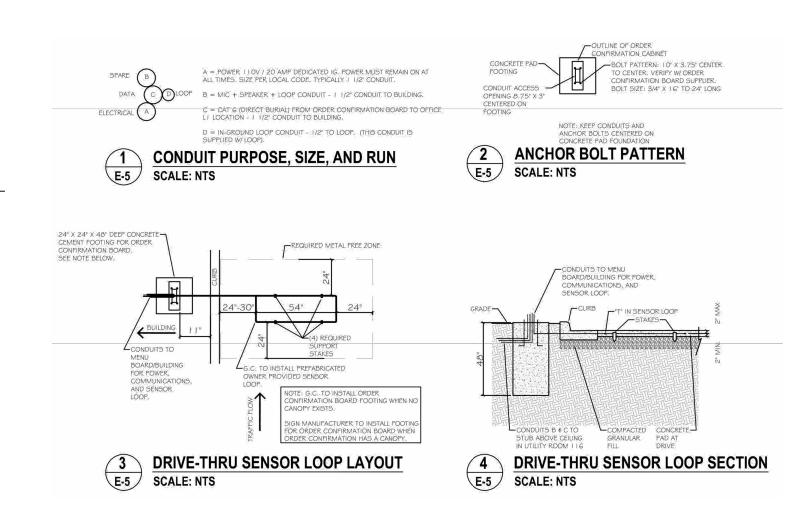


1. PROVIDE 1/2" EXPANSION JOINTS AT 20', MAXIMUM, SPACING AND FILLED WITH PREMOLDED BITUMINOUS EXPANSION JOINT FILLER MATERIAL OR REDWOOD. EXPANSION JOINTS SHALL HAVE #4 DOWELS, LUBRICATED, 18" LONG, AT 12" CENTERS, 6" FROM EDGE. 2. PROVIDE 3/8" GROOVED CONTROL JOINTS AT 5' CENTERS.

3. WELDED WIRE FABRIC (6X6-6X6) SHALL BE INSTALLED THROUGH DRIVEWAYS AT 2" ABOVE SLAB BOTTOM. 4. PROVIDE 1/2" BITUMINOUS EXPANSION JOINT FILLER MATERIAL WHERE WALK ABUTS EXISTING IMPROVEMENTS AND AT ALL CHANGES IN GRADE 5. USE 2-#4 REINFORCING BARS, 10' LONG OVER ALL UTILITY TRENCHES FOR NEW SIDEWALK AND CONNECTIONS

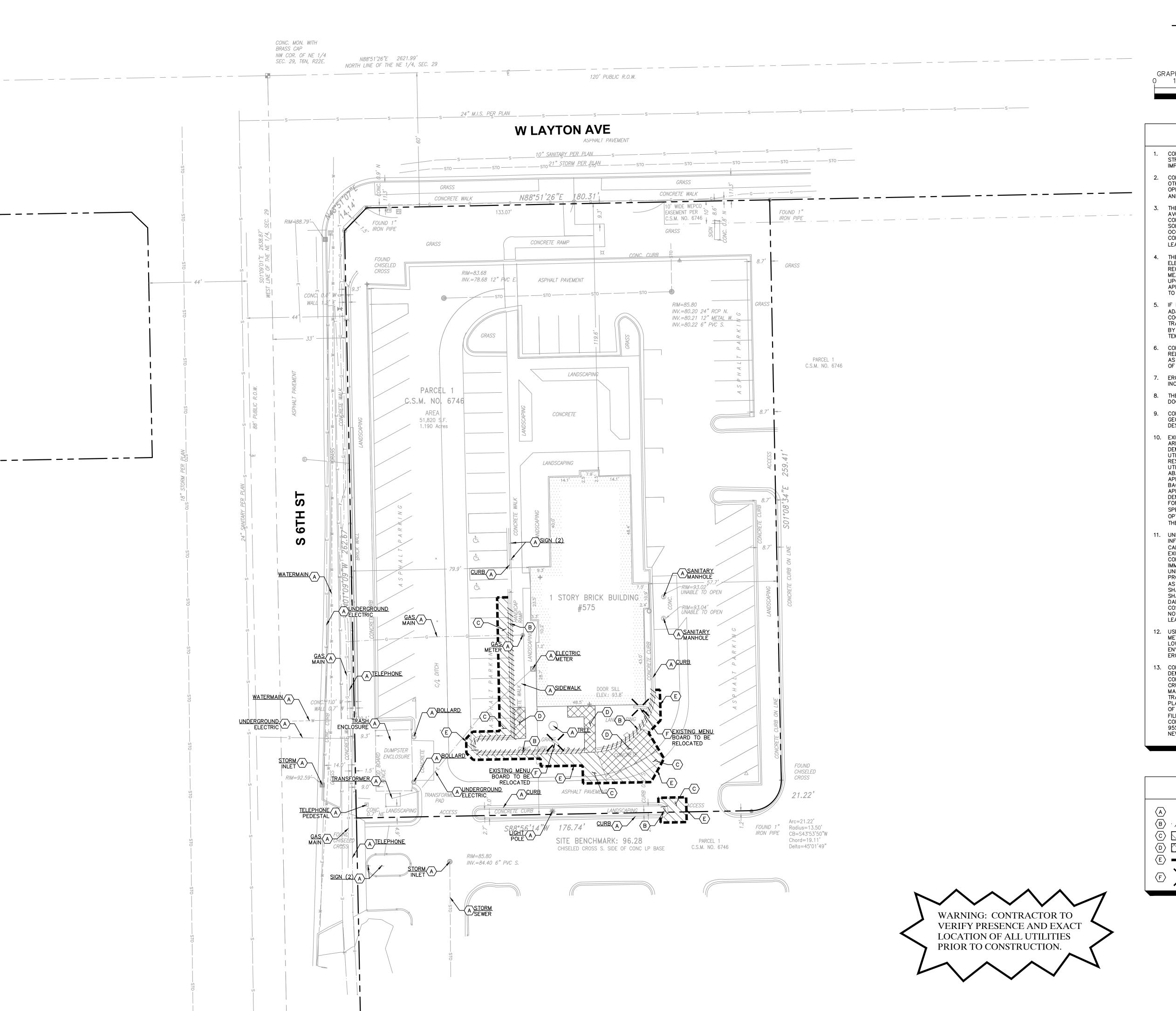
# **CONCRETE SIDEWALK**

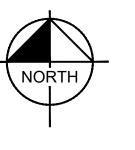
6. AT DRIVE APPROACHES, SIDEWALK PCC AND BASE THICKNESS SHALL MATCH THAT OF THE DRIVE.



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### **DEMOLITION NOTES**

- CONTRACTOR SHALL BE RESPONSIBLE FOR REMOVAL OF THE EXISTING STRUCTURES, RELATED UTILITIES, PAVING, AND ANY OTHER EXISTING IMPROVEMENTS AS NOTED.
- 2. CONTRACTOR IS TO REMOVE AND DISPOSE OF ALL DEBRIS, RUBBISH AND OTHER MATERIALS RESULTING FROM PREVIOUS AND CURRENT DEMOLITION OPERATIONS. DISPOSAL WILL BE IN ACCORDANCE WITH ALL LOCAL, STATE AND/OR FEDERAL REGULATIONS GOVERNING SUCH OPERATIONS.
- THE GENERAL CONTRACTOR SHALL TAKE ALL PRECAUTIONS NECESSARY TO AVOID PROPERTY DAMAGE TO ADJACENT PROPERTIES DURING THE CONSTRUCTION PHASES OF THIS PROJECT. THE CONTRACTOR WILL BE HELD SOLELY RESPONSIBLE FOR ANY DAMAGES TO THE ADJACENT PROPERTIES OCCURRING DURING THE CONSTRUCTION PHASES OF THIS PROJECT. CONTRACTOR SHALL NOT DEMOLISH ANYTHING OUTSIDE THE OWNERS LEASE/PROPERTY LINE UNLESS SPECIFICALLY MENTIONED ON THIS SHEET.
- THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR ELEVATION OF EXISTING UTILITIES AS SHOWN ON THESE PLANS IS BASED ON RECORDS OF THE VARIOUS UTILITY COMPANIES, AND WHERE POSSIBLE, MEASUREMENTS TAKEN IN THE FIELD. THE INFORMATION IS NOT TO BE RELIED UPON AS BEING EXACT OR COMPLETE. THE CONTRACTOR MUST CALL THE APPROPRIATE UTILITY COMPANY AT LEAST 72 HOURS BEFORE ANY EXCAVATION TO REQUEST EXACT FIELD LOCATION OF UTILITIES.
- 5. IF DEMOLITION OR CONSTRUCTION ON SITE WILL INTERFERE WITH THE ADJACENT PROPERTY OWNER'S TRAFFIC FLOW. THE CONTRACTOR SHALL COORDINATE WITH ADJACENT PROPERTY OWNER, TO MINIMIZE THE IMPACT ON TRAFFIC FLOW. TEMPORARY RE-ROUTING OF TRAFFIC IS TO BE ACCOMPLISHED BY USING WISDOT APPROVED TRAFFIC BARRICADES, BARRELS, AND/OR CONES. TEMPORARY SIGNAGE AND FLAGMEN MAY BE ALSO NECESSARY.
- 6. CONTRACTOR SHALL BEGIN CONSTRUCTION OF ANY LIGHT POLE BASES FOR RELOCATED LIGHT FIXTURES AND RELOCATION OF ELECTRICAL SYSTEM AS SOON AS DEMOLITION BEGINS. CONTRACTOR SHALL BE AWARE THAT INTERRUPTION OF POWER TO ANY LIGHT POLES OR SIGNS SHALL NOT EXCEED 24 HOURS.
- EROSION CONTROL MUST BE ESTABLISHED PRIOR TO ANY WORK ON SITE INCLUDING DEMOLITION.
- 8. THE EXTENT OF SITE DEMOLITION WORK IS AS SHOWN ON THE CONTRACT DOCUMENTS AND AS SPECIFIED HEREIN.
- 9. CONTRACTOR MUST RECEIVE APPROVAL FROM CIVIL ENGINEER AND GEOTECHNICAL ENGINEER FOR THE MATERIAL TYPE AND USE IF CONTRACTOR DESIRES TO REUSE DEMOLISHED SITE PAVEMENT AS STRUCTURAL FILL.
- 10. EXISTING UTILITIES, WHICH DO NOT SERVICE STRUCTURES BEING DEMOLISHED, ARE TO BE KEPT IN SERVICE AND PROTECTED AGAINST DAMAGE DURING DEMOLITION OPERATIONS. CONTRACTOR SHALL ARRANGE FOR SHUT-OFF OF UTILITIES SERVING STRUCTURES TO BE DEMOLISHED. CONTRACTOR IS RESPONSIBLE FOR TURNING OFF, DISCONNECTING, AND SEALING INDICATED UTILITIES BEFORE STARTING DEMOLITION OPERATIONS. EXISTING UTILITIES TO B ABANDONED ARE TO BE CAPPED AT BOTH ENDS AND FILLED WITH FA-1 OR APPROVED EQUAL. ALL UNDERGROUND UTILITIES TO BE REMOVED ARE TO BE BACKFILLED WITH ENGINEERED FILL OR SELECT EXCAVATED MATERIAL, AS APPROVED BY THE GEOTECHNICAL ENGINEER, TO 95% OF MODIFIED PROCTOR DENSITY WITHIN PAVED AREAS AND TO 90% OF MODIFIED PROCTOR DENSITY FOR GREEN SPACE AREAS, IN ACCORDANCE WITH THE EARTHWORK SPECIFICATIONS. ALL PRIVATE UTILITIES (ELECTRIC, CABLE, TELEPHONE, FIBER OPTIC, GAS) SHALL BE REMOVED AND RELOCATED PER THE UTILITY OWNER AND THE LOCAL MUNICIPALITY'S REQUIREMENTS.
- 11. UNDERGROUND UTILITIES SHOWN ARE BASED ON ATLASES AND AVAILABLE INFORMATION PRESENTED AT THE TIME OF SURVEY. CONTRACTOR SHOULD CALL 'DIGGERS" (1-800-242-8511) TO COORDINATE FIELD LOCATIONS OF EXISTING UNDERGROUND UTILITIES BEFORE ORDERING MATERIALS OR COMMENCING CONSTRUCTION. NOTIFY ENGINEER OF ANY DISCREPANCIES IMMEDIATELY. CONTRACTOR SHALL LOCATE AND PROTECT EXISTING UNDERGROUND AND OVERHEAD UTILITIES DURING CONSTRUCTION. UTILITY PROTECTION SHALL BE COORDINATED WITH THE RESPECTIVE UTILITY OWNER AND AS DIRECTED BY THE GOVERNING MUNICIPALITY. DAMAGED CABLES/CONDUITS SHALL BE REPLACED IMMEDIATELY. ALL EXISTING STRUCTURES TO REMAIN SHALL BE PROTECTED THROUGHOUT THE CONSTRUCTION PROCESS. ALL DAMAGED STRUCTURES SHALL BE REPLACED IN-KIND AND THEIR REPLACEMENT COST SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. PROPER NOTIFICATION TO THE OWNERS OF THE EXISTING UTILITIES SHALL BE MADE AT LEAST 48 HOURS BEFORE CONSTRUCTION COMMENCES.
- 12. USE WATER SPRINKLING, TEMPORARY ENCLOSURES, AND OTHER SUITABLE METHODS TO LIMIT DUST AND DIRT RISING AND SCATTERING IN THE AIR TO THI LOWEST LEVEL. COMPLY WITH ALL GOVERNING REGULATIONS PERTAINING TO ENVIRONMENTAL PROTECTION. SEE EROSION CONTROL SHEETS FOR FURTHER EROSION CONTROL REQUIREMENTS.
- 13. COMPLETELY FILL BELOW-GRADE AREAS AND VOIDS RESULTING FROM DEMOLITION OF STRUCTURES TO THE FINAL LINES AND GRADES SHOWN ON THE CONTRACT DOCUMENTS. BACKFILL MATERIAL SHALL BE WISDOT APPROVED CRUSHED LIMESTONE OR APPROVED EQUAL. USE SATISFACTORY SOIL MATERIALS CONSISTING OF STONE, GRAVEL AND SAND, FREE FROM DEBRIS. TRASH, FROZEN MATERIALS, ROOTS AND OTHER ORGANIC MATTER. PRIOR TO PLACEMENT OF FILL MATERIALS, ENSURE THAT AREAS TO BE FILLED ARE FREE OF STANDING WATER, FROST, FROZEN MATERIAL, TRASH AND DEBRIS. PLACE FILL MATERIALS IN HORIZONTAL LAYERS NOT EXCEEDING 9" IN LOOSE DEPTH. COMPACT EACH LAYER AT OPTIMUM MOISTURE CONTENT OF FILL MATERIAL TO 95% OF MODIFIED PROCTOR DENSITY UNLESS SUBSEQUENT EXCAVATION FOR NEW WORK IS REQUIRED.

#### **DEMOLITION LEGEND**

ITEM TO REMAIN, PROTECT DURING CONSTRUCTION B · //// CURB REMOVAL (C) FULL-DEPTH ASPHALT PAVEMENT REMOVAL D CONCRETE REMOVAL



EXISTING NDITIONS DEMO PLA

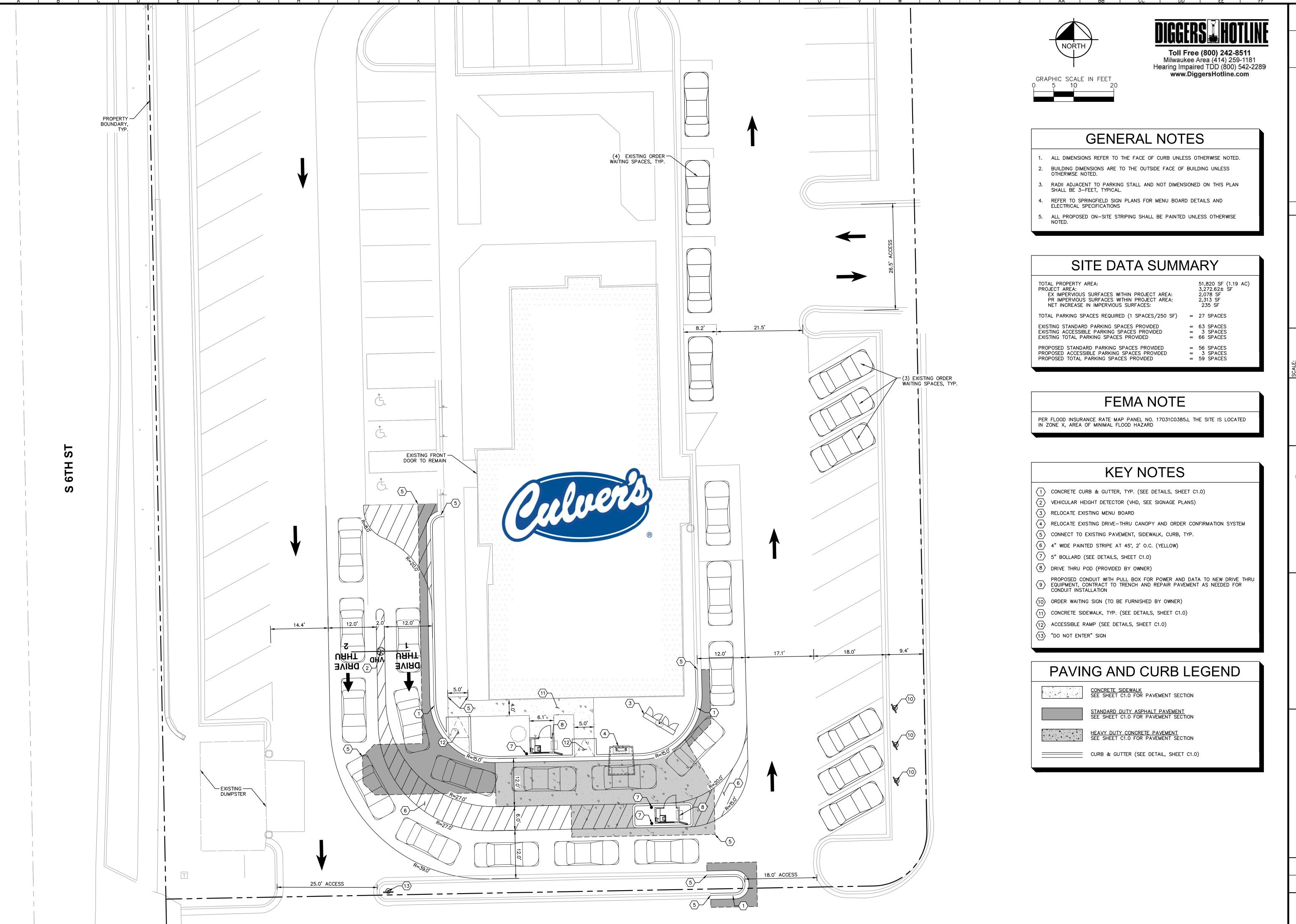
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Kimley

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REVISIONS

Kimley >> Horn
© 2023 KIMLEY-HORN AND ASSOCIATES, INC.
111 WEST JACKSON BOULEVARD, STE 1320
CHICAGO, IL 60604
PHONE: 312-726-9445
WWW.KIMLEY-HORN.COM

DRAWN BY: ANA CH CHECKED BY: JPM

BUTTERBURGERS & FROZEN GUSTARU

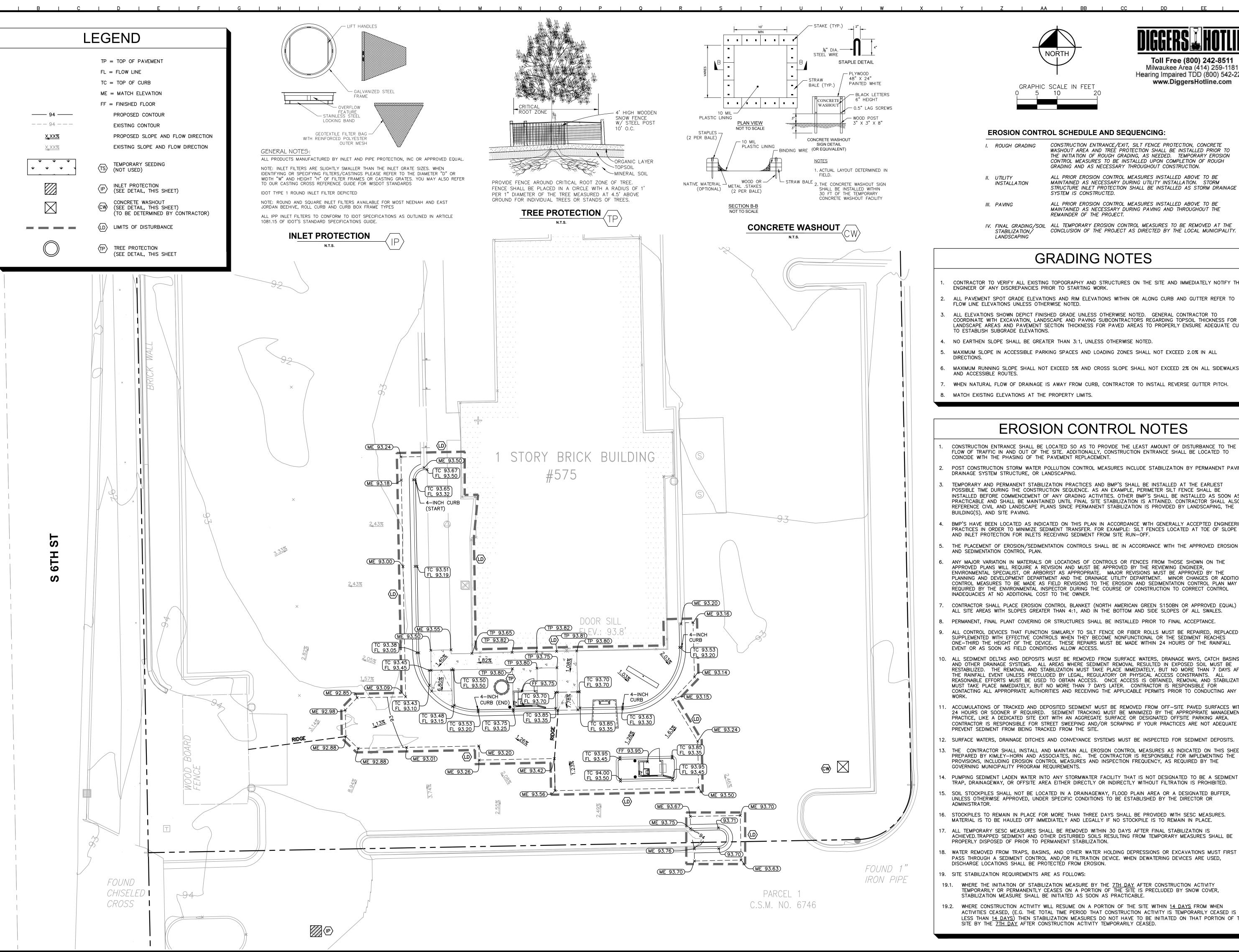
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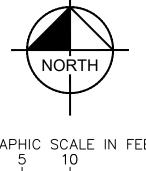
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#### **EROSION CONTROL SCHEDULE AND SEQUENCING:**

- CONSTRUCTION ENTRANCE/EXIT, SILT FENCE PROTECTION, CONCRETE WASHOUT AREA AND TREE PROTECTION SHALL BE INSTALLED PRIOR TO THE INITIATION OF ROUGH GRADING, AS NEEDED. TEMPORARY EROSION CONTROL MEASURES TO BE INSTALLED UPON COMPLETION OF ROUGH
  - ALL PRIOR EROSION CONTROL MEASURES INSTALLED ABOVE TO BE MAINTAINED AS NECESSARY DURING UTILITY INSTALLATION. STORM STRUCTURE INLET PROTECTION SHALL BE INSTALLED AS STORM DRAINAGE

GRADING AND AS NECESSARY THROUGHOUT CONSTRUCTION.

- ALL PRIOR EROSION CONTROL MEASURES INSTALLED ABOVE TO BE MAINTAINED AS NECESSARY DURING PAVING AND THROUGHOUT THE REMAINDER OF THE PROJECT.
- IV. FINAL GRADING/SOIL ALL TEMPORARY EROSION CONTROL MEASURES TO BE REMOVED AT THE CONCLUSION OF THE PROJECT AS DIRECTED BY THE LOCAL MUNICIPALITY.

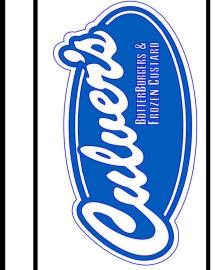
### **GRADING NOTES**

SYSTEM IS CONSTRUCTED.

- CONTRACTOR TO VERIFY ALL EXISTING TOPOGRAPHY AND STRUCTURES ON THE SITE AND IMMEDIATELY NOTIFY THE ENGINEER OF ANY DISCREPANCIES PRIOR TO STARTING WORK.
- 2. ALL PAVEMENT SPOT GRADE ELEVATIONS AND RIM ELEVATIONS WITHIN OR ALONG CURB AND GUTTER REFER TO FLOW LINE ELEVATIONS UNLESS OTHERWISE NOTED.
- ALL ELEVATIONS SHOWN DEPICT FINISHED GRADE UNLESS OTHERWISE NOTED. GENERAL CONTRACTOR TO COORDINATE WITH EXCAVATION, LANDSCAPE AND PAVING SUBCONTRACTORS REGARDING TOPSOIL THICKNESS FOR LANDSCAPE AREAS AND PAVEMENT SECTION THICKNESS FOR PAVED AREAS TO PROPERLY ENSURE ADEQUATE CUT
- 4. NO EARTHEN SLOPE SHALL BE GREATER THAN 3:1, UNLESS OTHERWISE NOTED.
- MAXIMUM SLOPE IN ACCESSIBLE PARKING SPACES AND LOADING ZONES SHALL NOT EXCEED 2.0% IN ALL
- MAXIMUM RUNNING SLOPE SHALL NOT EXCEED 5% AND CROSS SLOPE SHALL NOT EXCEED 2% ON ALL SIDEWALKS
- WHEN NATURAL FLOW OF DRAINAGE IS AWAY FROM CURB, CONTRACTOR TO INSTALL REVERSE GUTTER PITCH.
- 8. MATCH EXISTING ELEVATIONS AT THE PROPERTY LIMITS.

## **EROSION CONTROL NOTES**

- CONSTRUCTION ENTRANCE SHALL BE LOCATED SO AS TO PROVIDE THE LEAST AMOUNT OF DISTURBANCE TO THE FLOW OF TRAFFIC IN AND OUT OF THE SITE. ADDITIONALLY, CONSTRUCTION ENTRANCE SHALL BE LOCATED TO COINCIDE WITH THE PHASING OF THE PAVEMENT REPLACEMENT.
- POST CONSTRUCTION STORM WATER POLLUTION CONTROL MEASURES INCLUDE STABILIZATION BY PERMANENT PAVING, DRAINAGE SYSTEM STRUCTURE, OR LANDSCAPING.
- TEMPORARY AND PERMANENT STABILIZATION PRACTICES AND BMP'S SHALL BE INSTALLED AT THE EARLIEST POSSIBLE TIME DURING THE CONSTRUCTION SEQUENCE. AS AN EXAMPLE, PERIMETER SILT FENCE SHALL BE INSTALLED BEFORE COMMENCEMENT OF ANY GRADING ACTIVITIES. OTHER BMP'S SHALL BE INSTALLED AS SOON A PRACTICABLE AND SHALL BE MAINTAINED UNTIL FINAL SITE STABILIZATION IS ATTAINED. CONTRACTOR SHALL ALSO REFERENCE CIVIL AND LANDSCAPE PLANS SINCE PERMANENT STABILIZATION IS PROVIDED BY LANDSCAPING, THE
- BMP'S HAVE BEEN LOCATED AS INDICATED ON THIS PLAN IN ACCORDANCE WITH GENERALLY ACCEPTED ENGINEERING PRACTICES IN ORDER TO MINIMIZE SEDIMENT TRANSFER. FOR EXAMPLE: SILT FENCES LOCATED AT TOE OF SLOPE AND INLET PROTECTION FOR INLETS RECEIVING SEDIMENT FROM SITE RUN-OFF.
- THE PLACEMENT OF EROSION/SEDIMENTATION CONTROLS SHALL BE IN ACCORDANCE WITH THE APPROVED EROSION
- ANY MAJOR VARIATION IN MATERIALS OR LOCATIONS OF CONTROLS OR FENCES FROM THOSE SHOWN ON THE APPROVED PLANS WILL REQUIRE A REVISION AND MUST BE APPROVED BY THE REVIEWING ENGINEER, ENVIRONMENTAL SPECIALIST, OR ARBORIST AS APPROPRIATE. MAJOR REVISIONS MUST BE APPROVED BY THE PLANNING AND DEVELOPMENT DEPARTMENT AND THE DRAINAGE UTILITY DEPARTMENT. MINOR CHANGES OR ADDITIONAL CONTROL MEASURES TO BE MADE AS FIELD REVISIONS TO THE EROSION AND SEDIMENTATION CONTROL PLAN MAY BE REQUIRED BY THE ENVIRONMENTAL INSPECTOR DURING THE COURSE OF CONSTRUCTION TO CORRECT CONTROL
- CONTRACTOR SHALL PLACE EROSION CONTROL BLANKET (NORTH AMERICAN GREEN S150BN OR APPROVED EQUAL) ON ALL SITE AREAS WITH SLOPES GREATER THAN 4:1, AND IN THE BOTTOM AND SIDE SLOPES OF ALL SWALES.
- PERMANENT, FINAL PLANT COVERING OR STRUCTURES SHALL BE INSTALLED PRIOR TO FINAL ACCEPTANCE.
- ALL CONTROL DEVICES THAT FUNCTION SIMILARLY TO SILT FENCE OR FIBER ROLLS MUST BE REPAIRED, REPLACED OR SUPPLEMENTED WITH EFFECTIVE CONTROLS WHEN THEY BECOME NONFUNCTIONAL OR THE SEDIMENT REACHES ONE—THIRD THE HEIGHT OF THE DEVICE. THESE REPAIRS MUST BE MADE WITHIN 24 HOURS OF THE RAINFALL EVENT OR AS SOON AS FIELD CONDITIONS ALLOW ACCESS.
- 10. ALL SEDIMENT DELTAS AND DEPOSITS MUST BE REMOVED FROM SURFACE WATERS, DRAINAGE WAYS, CATCH BASINS AND OTHER DRAINAGE SYSTEMS. ALL AREAS WHERE SEDIMENT REMOVAL RESULTED IN EXPOSED SOIL MUST BE RESTABILIZED. THE REMOVAL AND STABILIZATION MUST TAKE PLACE IMMEDIATELY, BUT NO MORE THAN 7 DAYS AFTER THE RAINFALL EVENT UNLESS PRECLUDED BY LEGAL, REGULATORY OR PHYSICAL ACCESS CONSTRAINTS. ALL REASONABLE EFFORTS MUST BE USED TO OBTAIN ACCESS. ONCE ACCESS IS OBTAINED, REMOVAL AND STABILIZATION MUST TAKE PLACE IMMEDIATELY, BUT NO MORE THAN 7 DAYS LATER. CONTRACTOR IS RESPONSIBLE FOR CONTACTING ALL APPROPRIATE AUTHORITIES AND RECEIVING THE APPLICABLE PERMITS PRIOR TO CONDUCTING ANY
- ACCUMULATIONS OF TRACKED AND DEPOSITED SEDIMENT MUST BE REMOVED FROM OFF-SITE PAVED SURFACES WITHIN 24 HOURS OR SOONER IF REQUIRED. SEDIMENT TRACKING MUST BE MINIMIZED BY THE APPROPRIATE MANAGEMENT PRACTICE, LIKE A DEDICATED SITE EXIT WITH AN AGGREGATE SURFACE OR DESIGNATED OFFSITE PARKING AREA. CONTRACTOR IS RESPONSIBLE FOR STREET SWEEPING AND/OR SCRAPING IF YOUR PRACTICES ARE NOT ADEQUATE TO PREVENT SEDIMENT FROM BEING TRACKED FROM THE SITE.
- 12. SURFACE WATERS, DRAINAGE DITCHES AND CONVEYANCE SYSTEMS MUST BE INSPECTED FOR SEDIMENT DEPOSITS. THE CONTRACTOR SHALL INSTALL AND MAINTAIN ALL EROSION CONTROL MEASURES AS INDICATED ON THIS SHEET
- PREPARED BY KIMLEY-HORN AND ASSOCIATES, INC. THE CONTRACTOR IS RESPONSIBLE FOR IMPLEMENTING THE PROVISIONS, INCLUDING EROSION CONTROL MEASURES AND INSPECTION FREQUENCY, AS REQUIRED BY THE GOVERNING MUNICIPALITY PROGRAM REQUIREMENTS.
- TRAP, DRAINAGEWAY, OR OFFSITE AREA EITHER DIRECTLY OR INDIRECTLY WITHOUT FILTRATION IS PROHIBITED. 15. SOIL STOCKPILES SHALL NOT BE LOCATED IN A DRAINAGEWAY, FLOOD PLAIN AREA OR A DESIGNATED BUFFER,
- 16. STOCKPILES TO REMAIN IN PLACE FOR MORE THAN THREE DAYS SHALL BE PROVIDED WITH SESC MEASURES. MATERIAL IS TO BE HAULED OFF IMMEDIATELY AND LEGALLY IF NO STOCKPILE IS TO REMAIN IN PLACE.
- ALL TEMPORARY SESC MEASURES SHALL BE REMOVED WITHIN 30 DAYS AFTER FINAL STABILIZATION IS ACHIEVED.TRAPPED SEDIMENT AND OTHER DISTURBED SOILS RESULTING FROM TEMPORARY MEASURES SHALL BE PROPERLY DISPOSED OF PRIOR TO PERMANENT STABILIZATION.
- 18. WATER REMOVED FROM TRAPS, BASINS, AND OTHER WATER HOLDING DEPRESSIONS OR EXCAVATIONS MUST FIRST PASS THROUGH A SEDIMENT CONTROL AND/OR FILTRATION DEVICE. WHEN DEWATERING DEVICES ARE USED, DISCHARGE LOCATIONS SHALL BE PROTECTED FROM EROSION.
- 19. SITE STABILIZATION REQUIREMENTS ARE AS FOLLOWS:
- 19.1. WHERE THE INITIATION OF STABILIZATION MEASURE BY THE <u>7TH DAY</u> AFTER CONSTRUCTION ACTIVITY TEMPORARILY OR PERMANENTLY CEASES ON A PORTION OF THE SITE IS PRECLUDED BY SNOW COVER, STABILIZATION MEASURE SHALL BE INITIATED AS SOON AS PRACTICABLE.
- 19.2. WHERE CONSTRUCTION ACTIVITY WILL RESUME ON A PORTION OF THE SITE WITHIN 14 DAYS FROM WHEN ACTIVITIES CEASED, (E.G. THE TOTAL TIME PERIOD THAT CONSTRUCTION ACTIVITY IS TEMPORARILY CEASED IS LESS THAN 14 DAYS) THEN STABILIZATION MEASURES DO NOT HAVE TO BE INITIATED ON THAT PORTION OF THE SITE BY THE 7TH DAY AFTER CONSTRUCTION ACTIVITY TEMPORARILY CEASED.



orn

ORIGINAL ISSUE: 02/08/2023 KHA PROJECT NO 168974004

SHEET NUMBER

# WARNING: VERIFY WITH SITE PLAN AND FRANCHISEE IF PODS ARE TO BE INCLUDED IN PROJECT.

#### GENERAL NOTES:

- POD PROTOTYPE DRAWINGS ARE TO GIVE SCHMETIC DESIGN INTENT. FRANCHISEE IS RESPONSIBLE FOR ANY PERMITTING OR APPROVALS WITH CITY.
- 2. CONTRACTORS ARE RESPONSIBLE TO MAKE SURE DESIGN AND CONSTRUCTION IS COMPLIANT WITH LOCAL CODES AND CONSTRUCTION PRACTICES.
- 3. EXTERIOR FINISHES OF POD TO MATCH FINISHES USED ON RESTAURANT LOCATION.I

# DOOR SCHEDULE

DOOR:				FRAME:		HARDWARE		
NO.	<u>WIDTH</u>	HGT.	MAT'L	TYPE	MATERIAL	TYPE	<u>aroup</u>	<u>NOTES</u>
1	36"	76"	ALUM	F	ALUM	E	I	
la	2 @ 36"	76"	FG	G	ALUM	E	2	

NOTE: ONLY I DOOR PER POD. REF SITE PLAN AND POD PLAN TO VERIFY IF DOOR I OR Ia IS USED.

GROUP #	DOOR	<u>Q1Y</u>	DESCRIPTION	CATALOG #	FINISH	MFG	
			CONT. HINGE	224HD	628	IVES	
20 3₹ 0R			STORE RM LEVER SET	ND8ORD RHO	628	SCHLAGE	
POI ENTR			LOCK GUARD	LG12	630	IVES	
ш			SURFACE CLOSER	4111 SHCUSH	689	LCN	
			ARMOR PLATE	8400 36"x2"LDW B-C5	630	IVE5	
			GASKETING	429A	Α	ZERO	
			DOOR SWEEP	328AA	AA	ZERO	
			DOOR SWEEP	39A	Α	ZERO	
			THRESHOLD	65A-MSLA-10	Α	ZERO	
			RAIN DRIP	142A	Α	ZERO	
*N01ES	- MOUN	VT 420	9A HEAD SEAL PRIOR TO	O MOUNTING CLOSER		<u> </u>	 

WEATHER BARRIER WRAP

EXTEND PAST BASE

4 LOCATIONS

**POD - WALL SECTION** 

**POD** SCALE: 1-1/2" = 1'-0"

J'' PLYWOOD SHEATHING TO

- 2x5 STAINLESS STEEL ANGLE FOR

ANCHOR BOLT, BRACING REQ

MAY VARY BY LOCATION.

- DRIP SCREED AT BASE OF STONE FINISH

- ŽII DIA, HIL11 KWIK-HUS SCREW ANCHOR W/

MIN 3" EMBEDMENT INTO CONCRETE, TYP.

GROUP #	<u>1000</u> R	<u>Q1Y</u>	<u>DESCRIPTION</u>	CATALOG #	FINISH	MFG	
2	la		CONT. HINGE	112HD	628	IVES	
TE RY OR			DEAD BOLT LOCK				
RNATE ENTRY DOOR			RIM CYLINDER	20-057	628	SCHLAGE	
H			PUSH AND PULL SET				
ALTE			OH STOP	1005	630	GLYNN	
ច			SURFACE CLOSER	AIII EDA	689	LCN	
			MOUNTING PLATE	4110-18	689	LCN	
			BLADE STOP SPACER	4110-61	689	LCN	
			DOOR SWEEP	39A	А	ZERO	
			THRESHOLD	625A-MSLA-10	Α	ZERO	
*NOTE - PERIMETER WEATHER STRIPPING BY DOOR/ FRAME MFG.							

FRP OVER \$1" GYPSUM BOARD.

AND CEILING, TYP.

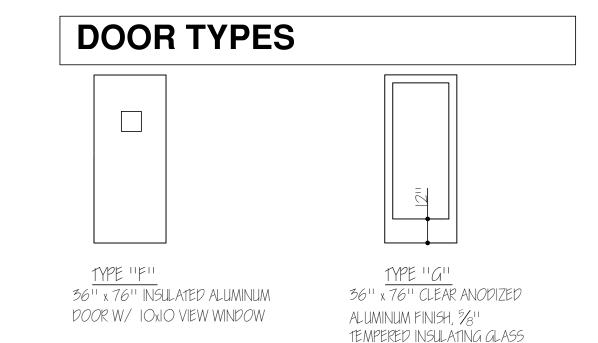
- R-15 BATT INSULATION AT WALLS

- 2x4 TREATED SUBFLOOR COVERED

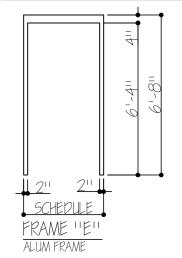
RIGID INSULATION BETWEEN FLOOR

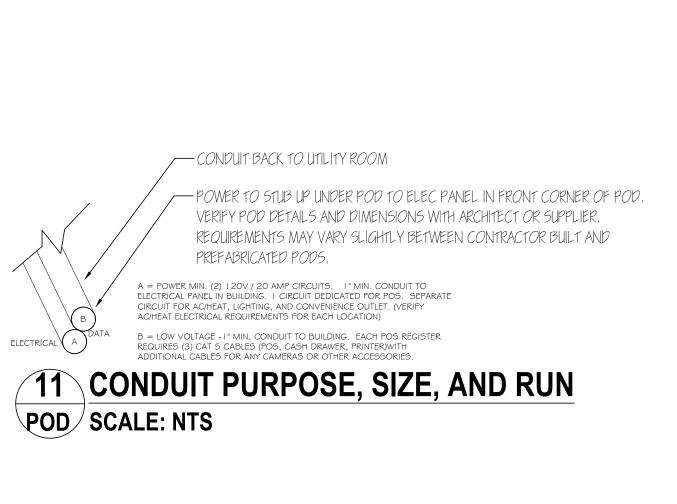
IN ½" PLYWOOD SHEATHING, 2"

- MARINE GRADE PLYWOOD BASE

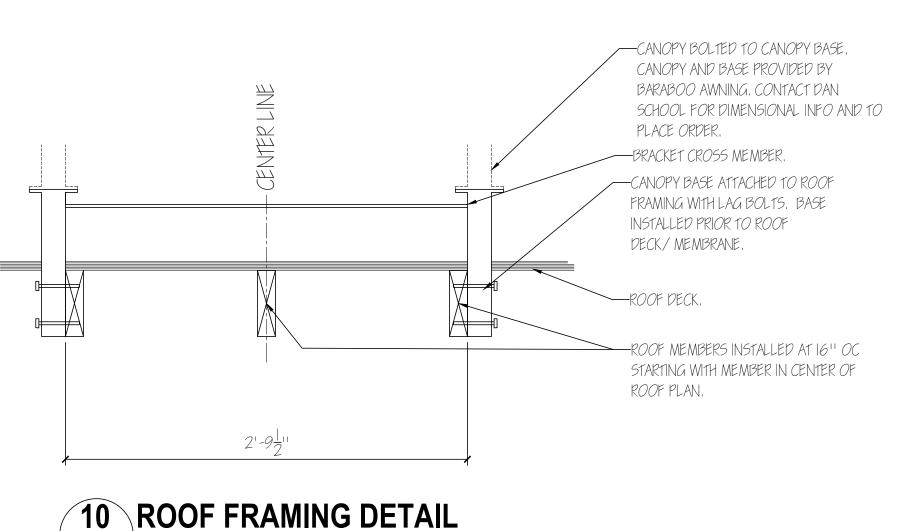


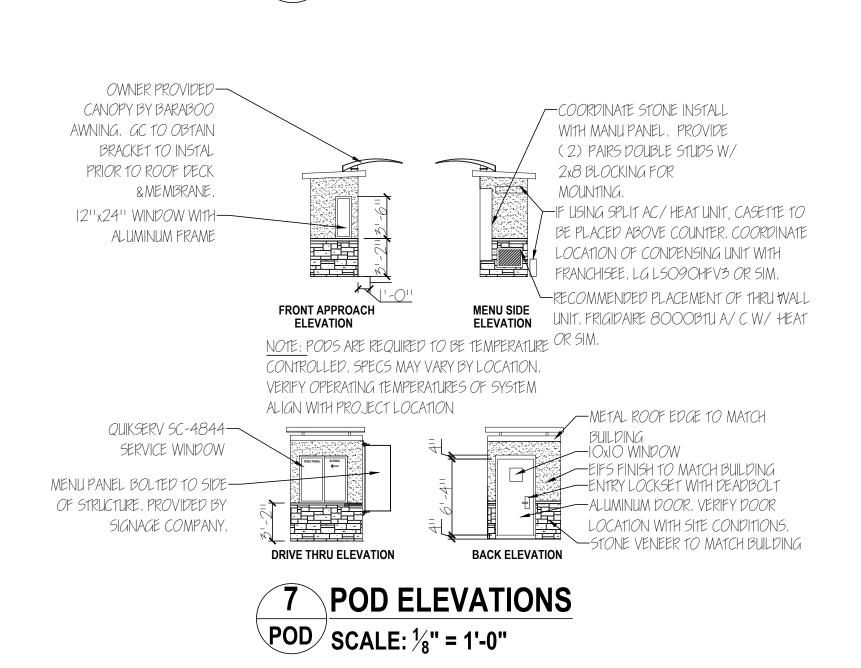
**FRAME TYPES** 





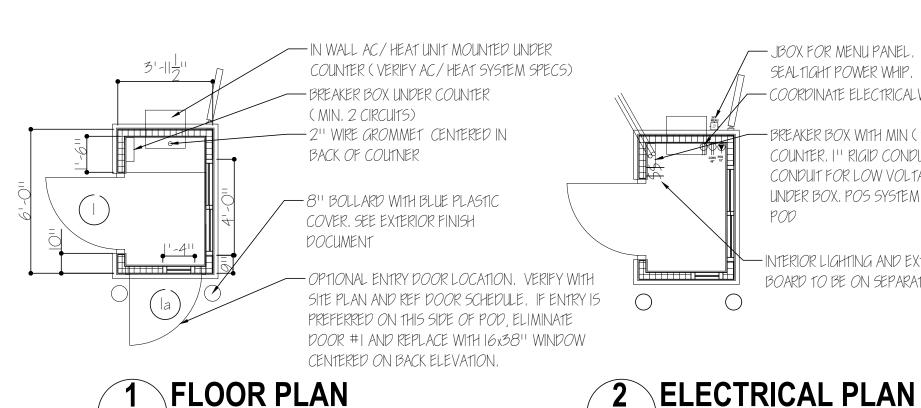
POD SCALE:  $1\frac{1}{2}$ " = 1'-0"



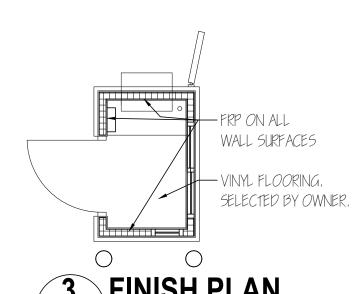


**8** POD SECTION

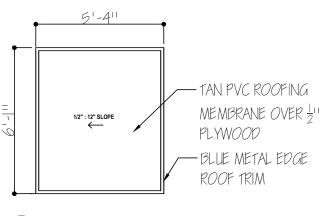
POD SCALE: 3/8" = 1'-0"



1 FLOOR PLAN POD SCALE: 1/4" = 1'-0"



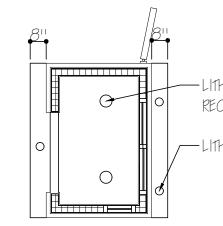
**3** FINISH PLAN POD SCALE: 1/4" = 1'-0"



**5** ROOF PLAN POD SCALE: 1/4" = 1'-0"

# BOARD TO BE ON SEPARATE SWITCHES INSIDE DOOR.

LITHONIA WF6 LED27K3OK35K 9OCRI BN



POD SCALE: 1/4" = 1'-0"

RECESSED CAN FIXTURE (QTY:2)

— JBOX FOR MENU PANEL. GC 10 PROVIDE 6'

REAKER BOX WITH MIN (2) 20 AMP BREAKERS UNDER

UNDER BOX. POS SYSTEM REQUIRES (3) CATS CABLES PER

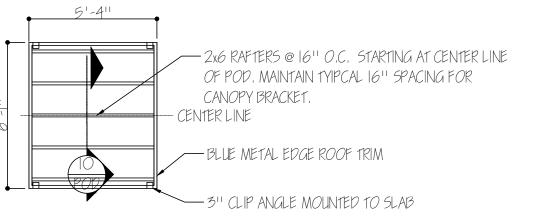
— INTERIOR LIGHTING AND EXTERIOR LIGHTING/MENU

— COORDINATE ELECTRICALW/ AC UNIT

SEALTIGHT POWER WHIP.

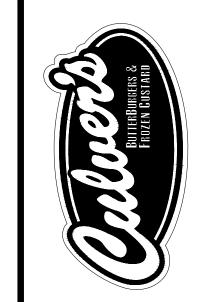
— LITHONIA LDNA 4" CAN LIGHT FIXTURE (QTY:3)

## 4 RCP/LIGHTING PLAN **POD** SCALE: 1/4" = 1'-0"



**6** ROOF FRAMING PLAN POD SCALE: 1/4" = 1'-0"

1240 Water Street 608-643-7980



DRIVE PROT

**PLANS** & DETAILS

> Metro L - 2023 R. KOTILA Jan. 1, 2023