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FINAL ENGINEERING PLANS

CULVER'S

575 W LAYTON AVE MILWAUKEE, WI 53207

UTILITY AND GOVERNING AGENCY CONTACTS

PLAN COMMISSION
 CITY OF MILWAUKEE
 809 NORTH BROADWAY, 2ND FLOOR
 MILWAUKEE, WI 53202
 TEL: (414) 286-5800
 CONTACT: STEPHANIE BLOOMINGDALE

SEWER AUTHORITY
 MILWAUKEE METROPOLITAN SEWERAGE
 DISTRICT (MMSD)
 260 W. SEEBOTH STREET
 MILWAUKEE, WI 53204
 TEL: (414) 272-5100

PUBLIC WORKS DEPARTMENT
 CITY OF MILWAUKEE
 841 NORTH BROADWAY, ROOM 501
 MILWAUKEE, WI 53202
 TEL: (515) 286-2489
 CONTACT: JERREL KRUSCHKE, P.E.

POWER COMPANY
 WE ENERGIES
 231 W. MICHIGAN STREET
 MILWAUKEE, WI 53203
 TEL: (800) 242-9137

FIRE DEPARTMENT
 CITY OF MILWAUKEE
 711 W WELLS STREET
 MILWAUKEE, WI 53233
 TEL: (414) 286-5213
 CONTACT: AARON LIPSKI, FIRE CHIEF

NATURAL GAS COMPANY
 WE ENERGIES
 231 W. MICHIGAN STREET
 MILWAUKEE, WI 53203
 TEL: (800) 242-9137

WATER SERVICE
 MILWAUKEE WATER WORKS
 841 N. BROADWAY, 4TH FLOOR
 MILWAUKEE, WI 53202
 TEL: (414) 286-2830

PROJECT TEAM

DEVELOPER
 NEO GEN, INC.
 3510 N. OAKLAND AVENUE, SUITE 210
 SHOREWOOD, WI 53211
 TEL: (414) 587-7459
 CONTACT: DIMITRI DIMITROPOULOS

CIVIL ENGINEER
 KIMLEY-HORN AND ASSOCIATES, INC.
 111 W. JACKSON BLVD.
 CHICAGO, IL 60604
 TEL: (630) 487-5550
 EMAIL: JOE.MAYER@KIMLEY-HORN.COM
 CONTACT: JOE MAYER
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 CONTACT: ANDY HEINEN, P.E.

SURVEYOR
 CHAPUT
 234 W. FLORIDA ST
 MILWAUKEE, WI 53204
 TEL: (414) 224-8068
 CONTACT: DONALD C. CHAPUT

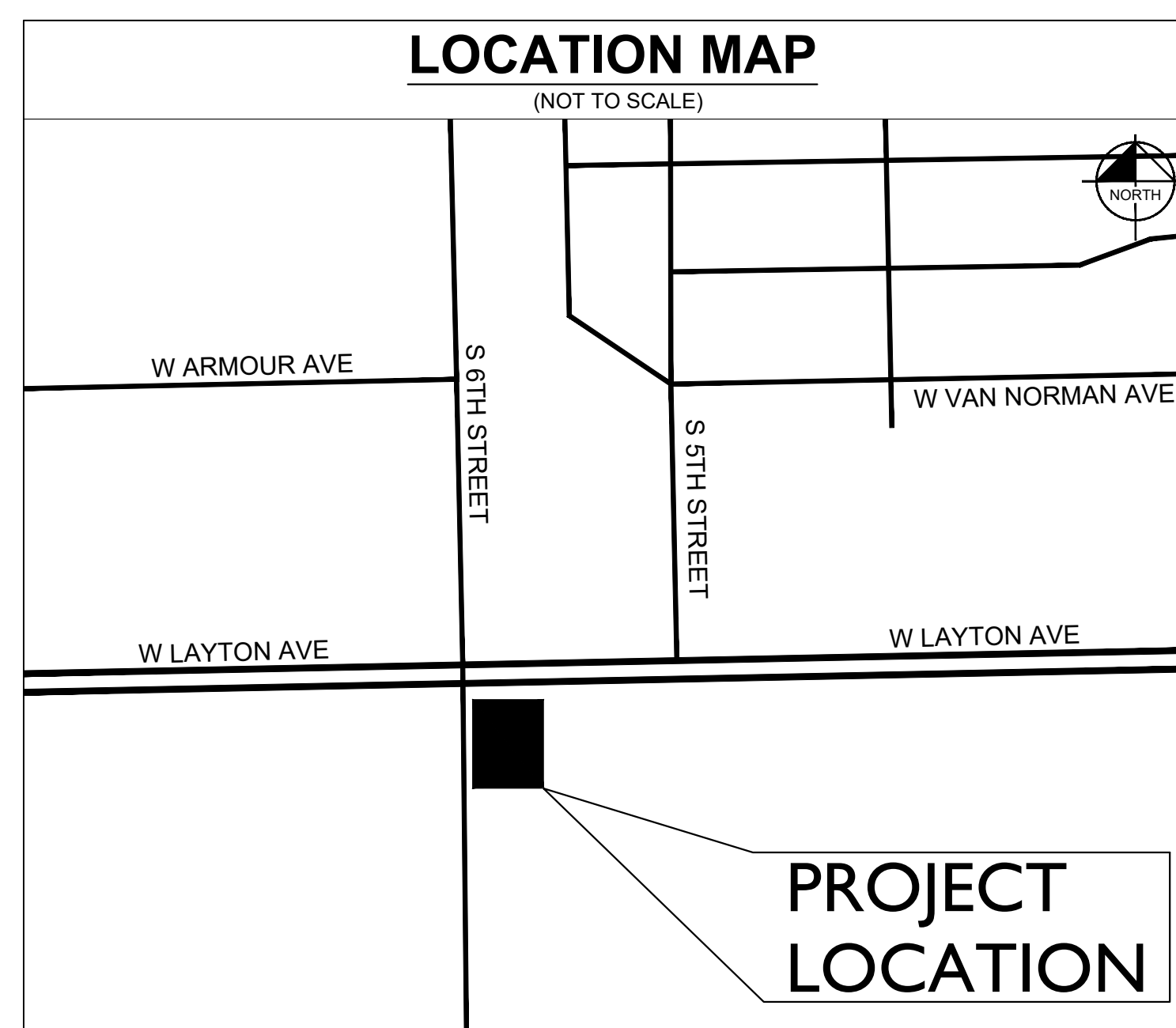
LANDSCAPE ARCHITECT
 KIMLEY-HORN AND ASSOCIATES, INC.
 4201 WINFIELD RD, SUITE 600
 WARRRENVILLE, IL 60555
 TEL: (630) 487-5550
 CONTACT: DANIEL GROVE, P.L.A.

LEGAL DESCRIPTION

PARCEL 2 OF CERTIFIED SURVEY MAP NO. 6746,
 RECORDED NOVEMBER 1, 1999 AS DOCUMENT NO.
 7837656, BEING A PART OF THE NORTHWEST 1/4 OF THE
 NORTHEAST 1/4 OF SECTION 29, TOWN 6 NORTH, RANGE
 22 EAST, IN THE CITY OF MILWAUKEE, MILWAUKEE
 COUNTY, WISCONSIN.

LOCATION MAP

(NOT TO SCALE)



BENCHMARKS

SITE BENCHMARKS:
 (LOCATIONS SHOWN ON SURVEY)
 VERTICAL DATUM IS BASED ON NATIONAL GEODETIC VERTICAL DATUM
 OF 1929 CONVERTED TO CITY OF MILWAUKEE VERTICAL DATUM.

(NGVD29) - (580.603) = CITY OF MILWAUKEE VERTICAL DATUM

SBM #1 REFERENCE MARK AND REFERENCE BENCHMARK CHISELED CROSS IN
 TOP OF W SIDE OF HYD.
 ELEVATION=90.676

SBM #2 CHISELED CROSS S. SIDE OF CONC LP BASE
 ELEVATION=96.28

Sheet List Table

Sheet Number	Sheet Title
C0.0	COVER SHEET
V0.0	TOPOGRAPHIC SURVEY (BY OTHERS)
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C2.0	EXISTING CONDITIONS AND DEMO PLAN
C3.0	SITE PLAN
C4.0	GRADING AND EROSION CONTROL PLAN

Kimley»Horn
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 111 WEST JACKSON BOULEVARD, STE 1320
 CHICAGO, IL 60604
 PHONE: 312-728-9445
 WWW.KIMLEY-HORN.COM



COVER SHEET

CULVER'S
 575 W LAYTON AVE
 MILWAUKEE, WI 53207

PROFESSIONAL ENGINEER'S CERTIFICATION

I, ANDREW HEINEN, A LICENSED PROFESSIONAL ENGINEER OF WI, HEREBY CERTIFY THAT THIS SUBMISSION, PERTAINING ONLY TO THE "C" SERIES CIVIL SHEETS LISTED ABOVE BUT EXCLUDING DETAILS PREPARED BY OTHERS, WAS PREPARED ON BEHALF OF NEO GEN, INC. BY KIMLEY-HORN AND ASSOCIATES, INC. UNDER MY PERSONAL DIRECTION. THIS TECHNICAL SUBMISSION IS INTENDED TO BE USED AS AN INTEGRAL PART OF AND IN CONJUNCTION WITH THE PROJECT SPECIFICATIONS AND CONTRACT DOCUMENTS.

DATED THIS 8TH DAY OF FEBRUARY, A.D., 2023.

A. Heinen

WI LICENSED PROFESSIONAL ENGINEER 36037-6
 MY LICENSE EXPIRES ON NOVEMBER 30, 2023
 DESIGN FIRM REGISTRATION NUMBER: 184002012-0006



Date of Expiration: 11-30-23

ORIGINAL ISSUE:
02/08/2023

KHA PROJECT NO.
168974004

SHEET NUMBER

C0.0

Drawing name: K:\GIS\DEV\168974004_Culvers_Milwaukee_WI2_Design\CAD\Plan\Sheets\C0.0 - COVER SHEET.dwg CO.0 Feb 08, 2023 3:18pm by: Brett Hickey
 This document, together with the concepts and designs presented herein, is intended only for the specific purpose and client for which it was prepared. Reuse of and improper reliance on this document without written authorization and adaptation by Kimley-Horn and Associates, Inc. shall be without liability to Kimley-Horn and Associates, Inc.

PLAT OF SURVEY WITH TOPOGRAPHIC MAP

CLIENT
Kimley-Horn

SITE ADDRESS

575 West Layton Avenue, City of Milwaukee, Milwaukee County, Wisconsin.

LEGAL DESCRIPTION

Parcel 2 of Certified Survey Map No. 6746, recorded November 1, 1999 as Document No. 7837656, being a part of the Northwest 1/4 of the Northeast 1/4 of Section 29, Town 6 North, Range 22 East, in the City of Milwaukee, Milwaukee County, Wisconsin.

BASIS OF BEARINGS

Bearings are referenced to the Wisconsin State Plane Coordinate System (South Zone), in which the North line of the NE 1/4 Section 29 bears N88°51'26"E.

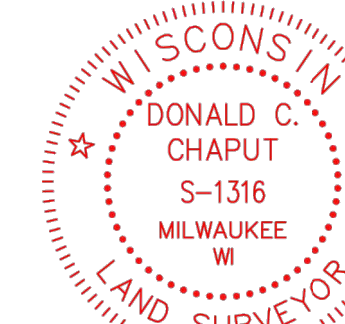
VERTICAL DATUM

Vertical datum is based on National Geodetic Vertical Datum of 1929 converted to City of Milwaukee Vertical Datum.
(NGVD29) - (-580.603) = City of Milwaukee Vertical Datum

VICINITY MAP



I Certify that I have surveyed the above described property (Property), and the above map is correct to the best of my professional knowledge and belief and shows the size and location of the Property, its exterior boundaries, the location and dimensions of all visible structures thereon, boundary fences, apparent easements and roadways and visible encroachments, if any. This survey is made for the exclusive use of the present owners of the Property, and also those who purchase, mortgage, or guarantee the title thereto, within one (1) year from date hereof.



Donald C. Chapat
Donald C. Chapat
Professional Land Surveyor
Registration Number S-1316

Date: September 15, 2011.

Date	Revision description

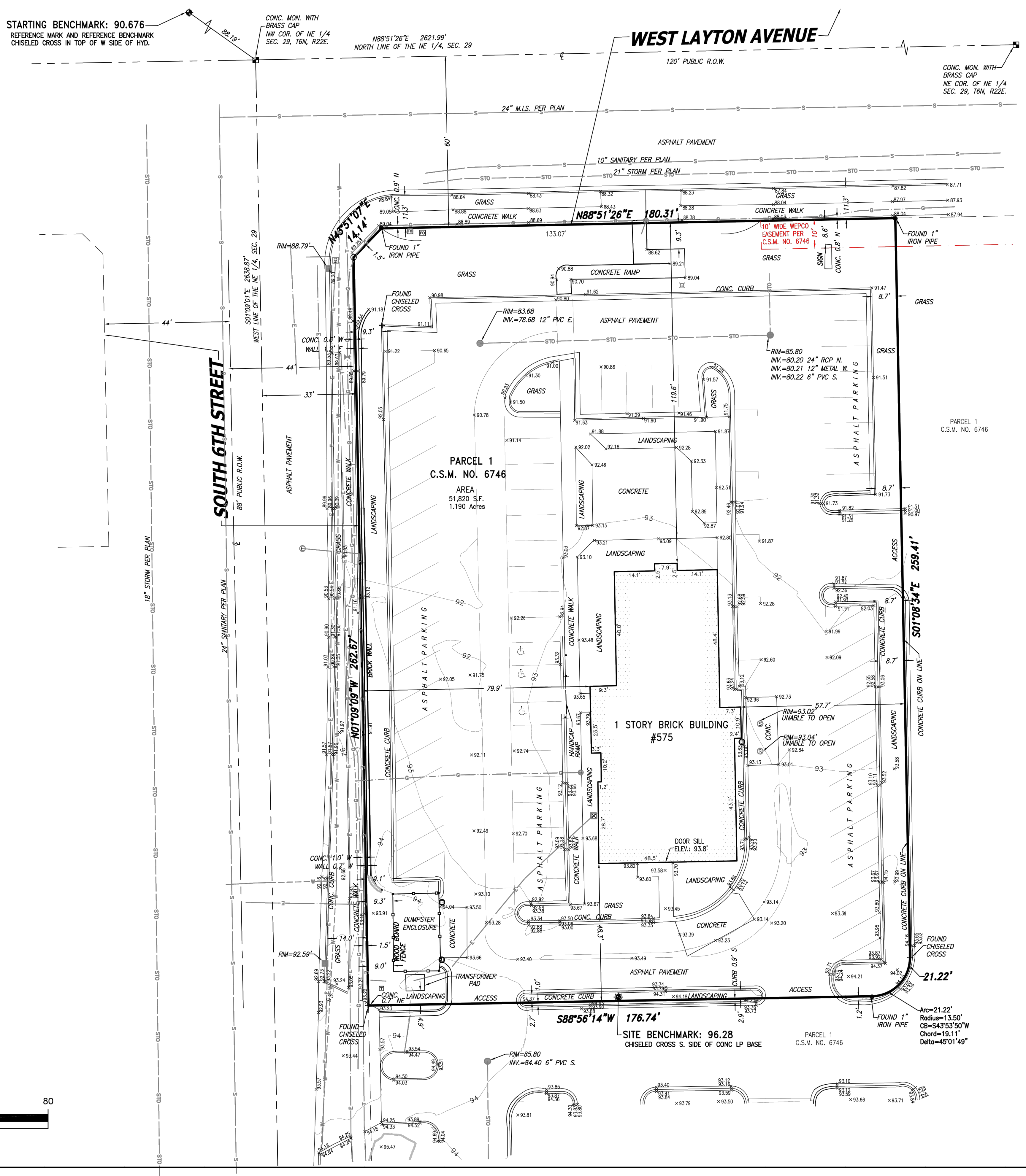
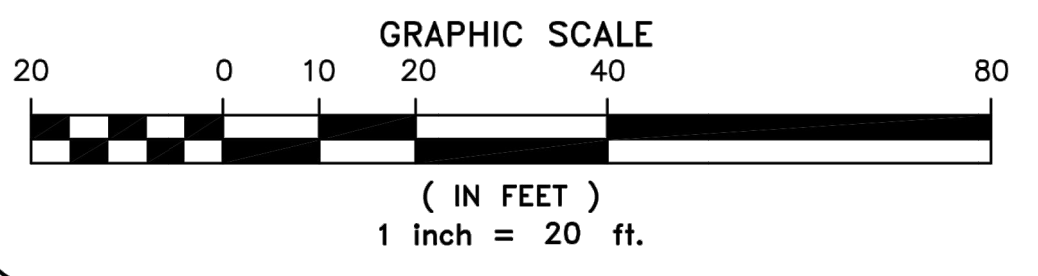
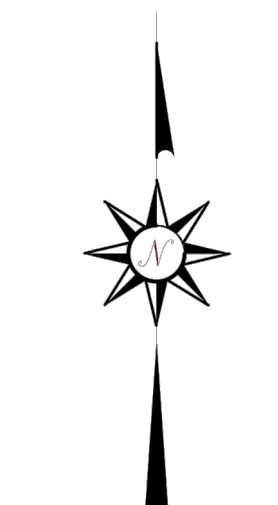
CHAPUT
LAND SURVEYS

234 W. Florida Street
Milwaukee, WI 53204

414-224-8068
www.chaputlandsurveys.com

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Drawing No. 3896-dzb

- LEGEND**
- INDICATES FOUND 1" IRON PIPE
 - INDICATES SET 1" IRON PIPE
 - + INDICATES FOUND CHISELED CROSS
 - SANITARY MANHOLE
 - ⊕ SANITARY CLEANOUT OR VENT
 - ⊙ SEPTIC TANK ACCESS COVER
 - ⊙ M.I.S. MANHOLE
 - ⊙ UNKNOWN MANHOLE
 - ⊙ STORM MANHOLE
 - INLET (ROUND)
 - ⊙ INLET (SQUARE)
 - ⊕ CURB INLET
 - ⊕ STORM SEWER END SECTION
 - ⊕ GAS VALVE
 - ⊕ GAS METER
 - ⊕ WATER VALVE
 - ⊕ HYDRANT
 - ⊕ WATER MANHOLE
 - ⊕ WATER SERVICE CURB STOP
 - ⊕ WELL HEAD
 - ⊕ STAND PIPE
 - ⊕ WALL INDICATOR VALVE
 - ⊕ POST INDICATOR VALVE
 - ⊕ LIGHT POLE
 - ⊕ SPOT/YARD LIGHT
 - ⊕ UTILITY POLE
 - ⊕ GUY POLE
 - ⊕ GUY WIRE
 - ⊕ ELECTRIC MANHOLE
 - ⊕ ELECTRIC PEDESTAL
 - ⊕ ELECTRIC METER
 - ⊕ TELEPHONE MANHOLE
 - ⊕ TELEPHONE PEDESTAL
 - ⊕ CABLE PEDESTAL
 - ⊕ CONTROL BOX
 - ⊕ FIBER OPTIC PEDESTAL/SIGN
 - ⊕ TRAFFIC LIGHT
 - ⊕ COMMUNICATION MANHOLE
 - ⊕ BOLLARD
 - ⊕ SOIL BORING/MONITORING WELL
 - ⊕ WATER SURFACE
 - ⊕ WETLANDS FLAG
 - ⊕ MARSH
 - ⊕ FLAGPOLE
 - ⊕ PARKING METER
 - ⊕ SIGN
 - ⊕ MAILBOX
 - ⊕ RAILROAD CROSSING SIGNAL
 - ⊕ HANDICAP SPACE
 - ⊕ CONFEROUS TREE
 - ⊕ DECIDUOUS TREE
- SANITARY SEWER
 - STORM SEWER
 - WATERLINE
 - MARKED GAS MAIN
 - MARKED ELECTRIC
 - OVERHEAD WIRES
 - MARKED TELEPHONE
 - MARKED CABLE TV LINE
 - MARKED FIBER OPTIC
 - BURIED ELECTRIC SERVICE
 - BOARD FENCE
 - CHAIN LINK FENCE
 - WIRE FENCE



CHAPUT LAND SURVEYS

GENERAL NOTES

- 1. EXISTING SITE TOPOGRAPHY, UTILITIES, RIGHT-OF-WAY AND HORIZONTAL CONTROL SHOWN ON THE DRAWINGS...
2. COPIES OF THE SURVEY ARE AVAILABLE FROM THE ENGINEER...
3. THE CONTRACTOR SHALL PHOTOGRAPH THE WORK AREA PRIOR TO CONSTRUCTION...
4. EXCEPT WHERE MODIFIED BY THE CONTRACT DOCUMENTS...
5. STANDARD SPECIFICATIONS, SUPPLEMENTAL SPECIFICATIONS, AND RECURRING SPECIAL PROVISIONS...
6. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO ENSURE THAT ALL ITEMS REQUIRED FOR CONSTRUCTION...
7. THE CONTRACTOR IS RESPONSIBLE FOR HAVING A SET OF "APPROVED" ENGINEERING PLANS WITH THE LATEST REVISION DATE...
8. THE CONTRACTOR SHALL SUBSCRIBE TO ALL GOVERNING REGULATIONS AND SHALL OBTAIN ALL NECESSARY PUBLIC AGENCY PERMITS PRIOR TO STARTING WORK...
9. THE ENGINEER AND OWNER ARE NOT RESPONSIBLE FOR THE CONSTRUCTION MEANS, METHODS, TECHNIQUES, SEQUENCES OF WORK OR PERFORMANCE PROGRAMS...
10. CONSTRUCTION MATERIALS AND/OR EQUIPMENT MAY NOT BE STORED IN THE RIGHT-OF-WAY...
11. EASEMENTS FOR THE EXISTING UTILITIES, BOTH PUBLIC AND PRIVATE, AND UTILITIES WITHIN PUBLIC RIGHT-OF-WAYS ARE SHOWN ON THE PLANS...
12. OWNER SHALL OBTAIN EASEMENTS AND APPROVAL OF PERMITS NECESSARY TO FACILITATE CONSTRUCTION OF THE PROPOSED UTILITIES...
13. THE CONTRACTOR SHALL PRESERVE ALL CONSTRUCTION STAKES UNTIL THEY ARE NO LONGER NEEDED...
14. NOTIFICATION OF COMMENCING CONSTRUCTION:
14.A. THE CONTRACTOR SHALL NOTIFY AFFECTED GOVERNMENTAL AGENCIES IN WRITING AT LEAST THREE FULL WORKING DAYS PRIOR TO COMMENCEMENT OF CONSTRUCTION...
14.B. FAILURE OF THE CONTRACTOR TO ALLOW PROPER NOTIFICATION TIME WHICH RESULTS IN THE TESTING CONTRACTOR'S WORK BEING TESTED UNDER A TIME SCHEDULE TESTING OPERATIONS...
15. ALL CONTRACTORS SHALL KEEP ACCESS AVAILABLE AT ALL TIMES FOR ALL EMERGENCY TRAFFIC...
16. ANY EXISTING SIGNS, LIGHT STANDARDS, AND UTILITY POLES THAT INTERFERE WITH CONSTRUCTION OPERATIONS ARE NOT AND NOTED ON THE PLANS FOR DISPOSAL...
17. ALL TREES TO BE SAVED SHALL BE IDENTIFIED PRIOR TO CONSTRUCTION BY THE LANDSCAPE ARCHITECT...
18. LIMB PRUNING SHALL BE PERFORMED UNDER THE SUPERVISION OF AN APPROVED LANDSCAPE ARCHITECT...
19. ALL EXISTING PAVEMENT OR CONCRETE TO BE REMOVED SHALL BE SAWCUT ALONG LIMITS OF PROPOSED REMOVAL...
20. ALL EXISTING UTILITIES OR IMPROVEMENTS, INCLUDING WALKS, CURBS, PAVEMENT, AND PARKWAYS DAMAGED OR REMOVED DURING CONSTRUCTION SHALL BE PROMPTLY RESTORED...
21. REMOVAL OF SPECIFIED ITEMS, INCLUDING BUT NOT LIMITED TO, PAVEMENT, SIDEWALK, CURB, CURB AND GUTTER, CULVERTS, ETC., SHALL BE DISPOSED OF OFF-SITE...
22. THE CONTRACTOR SHALL COLLECT AND REMOVE ALL CONSTRUCTION DEBRIS, EXCESS MATERIALS, TRASH, OIL AND GREASE RESIDUE, MAINTENANCE TOOLS, AND OTHER MISCELLANEOUS ITEMS...
23. NO UNDERGROUND WORK WITHIN THE PUBLIC RIGHT-OF-WAY SHALL BE COVERED UNTIL IT HAS BEEN APPROVED BY THE CITY OF MILWAUKEE...
24. WHERE SHOWN ON THE PLANS OR DIRECTED BY THE ENGINEER, EXISTING DRAINAGE STRUCTURES AND PIPE SHALL BE CLEANED OF DEBRIS AND PATCHED AS NECESSARY...
25. THE CONTRACTOR SHALL BE RESPONSIBLE FOR HAVING THE UTILITY COMPANIES LOCATE THEIR FACILITIES IN THE FIELD PRIOR TO CONSTRUCTION...
26. THE GENERAL CONTRACTOR SHALL COORDINATE WITH UTILITY COMPANIES TO PROVIDE CABLE TV, PHONE, ELECTRIC, GAS AND IRRIGATION SERVICES...
27. CONTRACTOR IS TO VERIFY ALL EXISTING STRUCTURES AND FACILITIES AT ALL PROPOSED UTILITY CONNECTION LOCATIONS...
28. ANY FIELD TILES ENCOUNTERED SHALL BE INSPECTED BY THE ENGINEER...
29. ALL FRAMES AND LIDS FOR STORM AND SANITARY SERVICES, VALVE VALVE COVERS, FIRE HYDRANTS, AND B-BOXES ARE TO BE ADJUSTED TO MEET FINISHED GRADE...
30. HYDRANTS SHALL NOT BE FLUSHED DIRECTLY ONTO THE ROAD SURFACES...

- 31. TRENCH BACKFILL WILL BE REQUIRED TO THE FULL DEPTH ABOVE SEWERS AND WATERMAIN WITHIN TWO (2) FEET HORIZONTAL OF PROPOSED OR EXISTING PAVEMENT.
32. IF SOFT, SPONGY, OR OTHER UNSUITABLE SOILS WITH UNCONFINED COMPRESSIVE STRENGTH LESS THAN 0.5 TSF ARE ENCOUNTERED AT THE BOTTOM OF THE TRENCH...
33. THE TRENCHES FOR PIPE INSTALLATION SHALL BE KEPT DRY AT ALL TIMES DURING PIPE PLACEMENT...
34. AFTER THE STORM SEWER SYSTEM HAS BEEN CONSTRUCTED, THE CONTRACTOR SHALL PLACE PROPER INLET PROTECTION EROSION CONTROL...
35. AT THE CLOSE OF EACH WORKING DAY AND AT THE CONCLUSION OF CONSTRUCTION OPERATIONS...
36. EROSION CONTROL MEASURES SHALL BE INSTALLED IN ACCORDANCE WITH THE WISCONSIN DEPARTMENT OF NATURAL RESOURCES REGULATIONS...
37. THE CONTRACTOR SHALL CONFORM TO ALL EROSION CONTROL REQUIREMENTS AS SET FORTH BY THE WISCONSIN DEPARTMENT OF NATURAL RESOURCES...
38. THE PAVEMENT SHALL BE KEPT FREE OF MUD AND DEBRIS AT ALL TIMES...
39. ALL DISTURBED AREAS OF THE RIGHT-OF-WAY SHALL BE FULLY RESTORED TO PRE-CONSTRUCTION CONDITIONS...
40. ALL PROPOSED GRADES SHOWN ON PLANS ARE FINISHED SURFACE ELEVATIONS...
41. ALL TESTING SHALL BE THE RESPONSIBILITY AND EXPENSE OF THE CONTRACTOR...
42. PROVIDE SMOOTH VERTICAL CURVES THROUGH HIGH AND LOW POINTS INDICATED BY SPOT ELEVATIONS...
43. WHEN REQUIRED, THE CONTRACTOR SHALL NOTIFY THE OWNER WHEN RECORD DRAWINGS CAN BE PREPARED...
44. BEFORE ACCEPTANCE, ALL WORK SHALL BE INSPECTED BY THE CITY OF MILWAUKEE, AS NECESSARY.

EARTHWORK NOTES

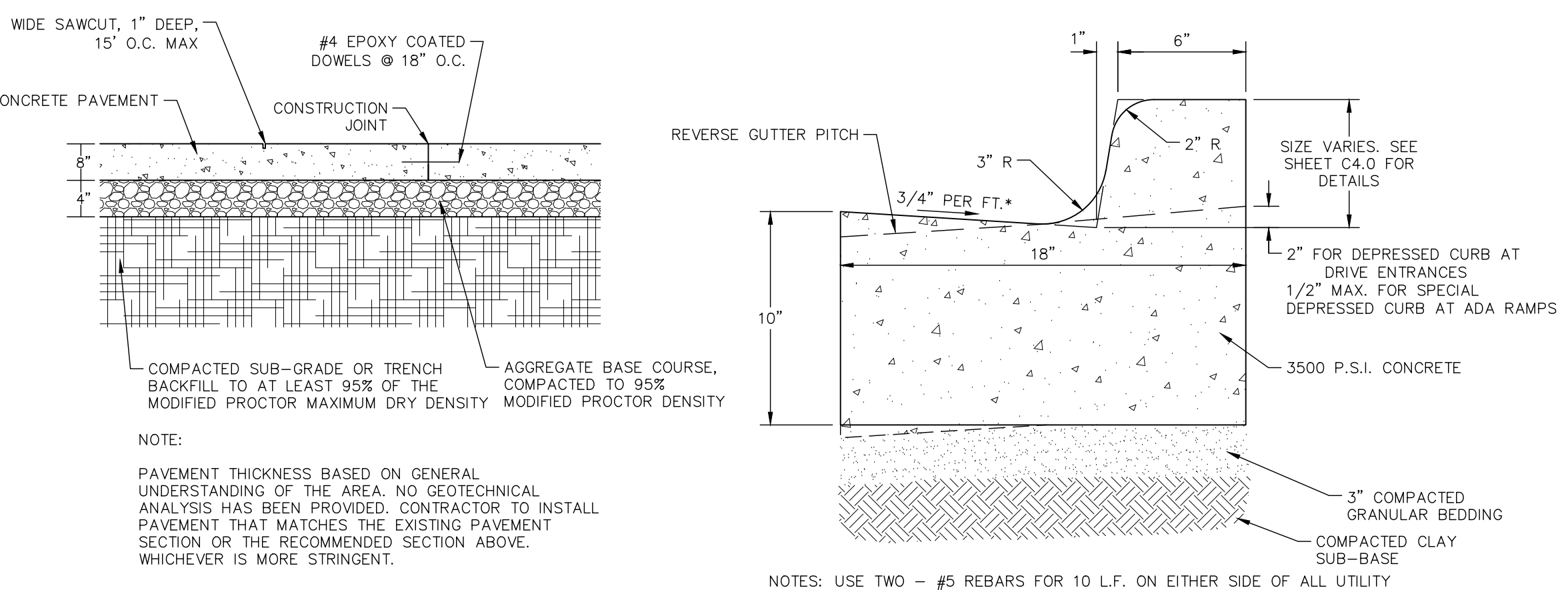
- 1. GENERAL
1.1. IT IS THE CONTRACTOR'S RESPONSIBILITY TO UNDERSTAND THE SOIL AND GROUNDWATER CONDITIONS AT THE SITE.
1.2. ANY QUANTITIES IN THE BID PROPOSAL ARE INTENDED AS A GUIDE FOR THE CONTRACTOR'S USE IN DETERMINING THE SCOPE OF THE COMPLETED PROJECT...
1.3. THE CONTRACTOR WILL NOTE THAT THE ELEVATIONS SHOWN ON THE CONSTRUCTION PLANS ARE FINISHED GRADE AND THAT PAVEMENT THICKNESS, TOPSOIL, ETC., MUST BE ACCOUNTED FOR.
1.4. THE CONTRACTOR SHALL MAINTAIN POSITIVE DRAINAGE DURING CONSTRUCTION AND PREVENT STORMWATER FROM RUNNING INTO OR STANDING IN EXCAVATED AREAS...
1.5. THE CONTRACTOR SHALL BE RESPONSIBLE FOR IMPLEMENTATION OF THE SOIL EROSION AND SEDIMENTATION CONTROL MEASURES...
1.6. PRIOR TO COMMENCEMENT OF GRADING ACTIVITIES, THE CONTRACTOR SHALL ERECT A CONSTRUCTION FENCE AROUND ANY TREE DESIGNATED TO BE PRESERVED...
2. TOPSOIL EXCAVATION INCLUDES:
2.1. EXCAVATION OF TOPSOIL AND OTHER STRUCTURALLY UNSUITABLE MATERIALS WITHIN THOSE AREAS THAT WILL REQUIRE EARTH EXCAVATION OR COMPACTED EARTH FILL MATERIAL...
2.2. PLACEMENT OF EXCAVATED MATERIAL IN OWNER-DESIGNATED AREAS FOR FUTURE USE...
2.3. TOPSOIL STOCKPILED FOR RESPAVED SHALL BE FREE OF CLAY AND SHALL NOT CONTAIN ANY OF THE TRANSITIONAL MATERIAL BETWEEN THE TOPSOIL AND CLAY...
2.4. TOPSOIL RESPAVED SHALL INCLUDE HAULING AND SPREADING SIX (6) INCHES OF TOPSOIL DIRECTLY OVER AREAS TO BE LANDSCAPED...
3. EARTH EXCAVATION INCLUDES:
3.1. EXCAVATION OF SUBSURFACE MATERIALS WHICH ARE SUITABLE FOR USE AS STRUCTURAL FILL...
3.2. PLACEMENT OF SUITABLE MATERIALS SHALL BE WITHIN THOSE AREAS REQUIRING STRUCTURAL FILL...
3.3. STRUCTURAL FILL MATERIAL MAY BE PLACED WITHIN THOSE PORTIONS OF THE SITE NOT REQUIRING STRUCTURAL FILL...
3.4. COMPACTION OF SUITABLE MATERIALS SHALL BE TO AT LEAST 93% OF THE MODIFIED PROCTOR DRY DENSITY...
5. MISCELLANEOUS. THE CONTRACTOR SHALL:
5.1. SPREAD AND COMPACT UNIFORMLY TO THE DEGREE SPECIFIED ALL EXCESS TRENCH SOIL AFTER COMPLETION OF THE UNDERGROUND IMPROVEMENTS.
5.2. SCARIFY, DISC, AERATE, AND COMPACT, TO THE DEGREE SPECIFIED, THE UPPER TWELVE (12) INCHES OF THE SUITABLE SUBGRADE MATERIAL...
5.3. PROVIDE WATER TO ADD TO DRY MATERIAL IN ORDER TO ADJUST THE MOISTURE CONTENT FOR THE PURPOSE OF ACHIEVING THE SPECIFIED COMPACTION.
5.4. BACKFILL THE CURB AND GUTTER AFTER ITS CONSTRUCTION AND PRIOR TO THE PLACEMENT OF THE BASE COURSE MATERIAL.
6. TESTING AND FINAL ACCEPTANCE
6.1. THE CONTRACTOR SHALL PROVIDE AS A MINIMUM A FULLY LOADED SIX-WHEEL TRUCK FOR PROOF ROLLING THE PAVEMENT AND CURB AND GUTTER...
6.2. ANY UNSUITABLE AREA ENCOUNTERED AS A RESULT OF PROOF ROLLING SHALL BE REMOVED AND REPLACED WITH SUITABLE MATERIAL OR OTHERWISE CORRECTED AND APPROVED BY THE ENGINEER.

SIGNAGE AND PAVEMENT MARKING NOTES

- 1. ALL SIGNING AND PAVEMENT MARKING SHALL BE IN ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) AND THE WISCONSIN DEPARTMENT OF TRANSPORTATION (WISDOT) STANDARDS.
2. SIGNS: SIGNS SHALL BE CONSTRUCTED OF 0.080-INCH THICK FLAT ALUMINUM PANELS WITH REFLECTORIZED LEGEND ON THE FACE...
3. POSTS: SIGN POSTS SHALL BE A HEAVY-DUTY STEEL "U" SHAPED CHANNEL WEIGHING 3.0 POUNDS/FOOT...
4. SIGNS AND POSTS SHALL BE INSTALLED IN ACCORDANCE WITH WISDOT STANDARDS.
5. PAVEMENT MARKINGS: ALL PAVEMENT MARKINGS IN THE PUBLIC RIGHT-OF-WAY, SUCH AS STOP LINES, CENTERLINES, CROSSWALKS, AND DIRECTIONAL ARROWS...
6. PAVEMENT MARKINGS ON BIKE PATHS, PARKING LOT STALLS, AND REPLICATOR "LOW-WEAR" APPLICATIONS, SHALL BE PAINT IN ACCORDANCE WITH WISDOT STANDARDS.
7. COLOR, WIDTH, STYLE, AND SIZE OF ALL MARKINGS SHALL BE IN ACCORDANCE WITH THE MUTCD AND LOCAL CODE.
8. THERMOPLASTIC MARKINGS SHALL BE INSTALLED WHEN THE PAVEMENT TEMPERATURE IS 55 DEGREES FAHRENHEIT AND RISINGS OR MARKINGS MAY BE INSTALLED WHEN THE AIR TEMPERATURE IS 50 DEGREES FAHRENHEIT AND RISING.

PAVING NOTES

- 1. GENERAL
1.1. PAVING WORK INCLUDES FINAL SUBGRADE SHAPING, PREPARATION, AND COMPACTION; PLACEMENT OF SUBGRADE OR BASE COURSE MATERIALS; BITUMINOUS BINDER AND/OR SURFACE COURSES; FORMING, FINISHING, AND CURING CONCRETE PAVEMENT, CURBS, AND WALKS; AND FINAL CLEAN-UP AND ALL RELATED WORK.
1.2. COMPACTION REQUIREMENTS [REFERENCE ASTM D-1557 (MODIFIED PROCTOR)] SUBGRADE = 93%; SUBGRADE = 93% AGGREGATE BASE COURSE = 95% BITUMINOUS COURSES = 95% OF MAXIMUM DENSITY PER WISCONSIN DEPARTMENT OF TRANSPORTATION (WISDOT) HIGHWAY STANDARDS.
1.3. IT SHALL BE THE CONTRACTOR'S SOLE RESPONSIBILITY TO PROVIDE PROPER BARRICADING WARNING DEVICES AND THE SAFE MANAGEMENT OF TRAFFIC WITHIN THE AREA OF CONSTRUCTION...
2. SUBGRADE PREPARATION
2.1. EARTHWORK FOR PROPOSED PAVEMENT SUBGRADE SHALL BE FINISHED TO WITHIN 0.1 FOOT, PLUS OR MINUS OF PLAN ELEVATION...
2.2. PRIOR TO THE PLACEMENT OF THE BASE COURSE, THE SUBGRADE MUST BE PROOF-ROLLED AND INSPECTED FOR UNSUITABLE MATERIALS AND/OR EXCESSIVE MOVEMENT...
2.2.1. SCARIFY, DISC, AND AERATE.
2.2.2. REMOVE AND REPLACE WITH STRUCTURAL CLAY FILL.
2.2.3. REMOVE AND REPLACE WITH GRANULAR MATERIAL.
2.2.4. USE OF GEOTEXTILE FABRIC.
2.2.5. MAXIMUM DEFLECTION ALLOWED IN ISOLATED AREAS MAY BE ONE-QUARTY (1/4) INCH TO ONE-HALF (1/2) INCH IF NO DEFLECTION OCCURS OVER THE MAJORITY OF THE AREA.
3. CONCRETE WORK
3.1. ALL EXTERIOR CONCRETE SHALL BE PORTLAND CEMENT CONCRETE WITH AIR ENTRAINMENT OF NOT LESS THAN FIVE (5) OR MORE THAN EIGHT (8) PERCENT...
3.2. CONCRETE CURB AND/OR COMBINATION CURB AND GUTTER SHALL BE OF THE TYPE SHOWN ON THE PLANS...
3.3. CURBS SHALL BE DEPRESSED AND MEET THE SLOPE REQUIREMENTS OF THE FEDERAL ADA STANDARDS...
3.4. THE CURBS SHALL BE BACKFILLED AFTER THEIR CONSTRUCTION AND PRIOR TO THE PLACEMENT OF THE BASE COURSE...
3.5. CONCRETE SIDEWALK SHALL BE IN ACCORDANCE WITH THE ABOVE AND THE PLANS...
3.6. CONCRETE CURING AND PROTECTION SHALL BE PER WISDOT STANDARDS...
3.7. THE COST OF AGGREGATE BASE OR SUBGRADE UNDER CONCRETE WORK SHALL BE INCLUDED IN THE COST OF THE RESPECTIVE CONTRACT ITEM.
4. FLEXIBLE PAVEMENT
4.1. THE PAVEMENT MATERIALS FOR BITUMINOUS STREETS, PARKING LOTS, AND DRIVE AISLES SHALL BE AS DETAILED ON PLANS...
4.2. ALL TRAFFIC SHALL BE KEPT OFF THE COMPLETED AGGREGATE BASE UNTIL THE BINDER COURSE IS LAID...
4.3. PRIOR TO PLACEMENT OF THE SURFACE COURSE, THE BINDER COURSE SHALL BE CLEANED AND TACK-COATED IF DUSTY OR DIRTY...
4.4. SEAMS IN BAM, BINDER, AND SURFACE COURSE SHALL BE STAGGERED A MINIMUM OF 6 INCHES.
5. TESTING AND FINAL ACCEPTANCE
5.1. THE CONTRACTOR SHALL FOLLOW THE QUALITY CONTROL TESTING PROGRAM FOR CONCRETE AND PAVEMENT MATERIALS ESTABLISHED BY THE ENGINEER.
5.2. PRIOR TO PLACEMENT OF THE BITUMINOUS CONCRETE SURFACE COURSE, THE CONTRACTOR, WHEN REQUIRED BY THE CITY OF MILWAUKEE, SHALL OBTAIN SPECIMENS OF THE BINDER COURSE WITH A CORE DRILL...
5.3. WHEN REQUIRED BY THE CITY OF MILWAUKEE, THE CONTRACTOR SHALL OBTAIN SPECIMENS OF THE FULL DEPTH BITUMINOUS CONCRETE PAVEMENT STRUCTURE...
5.4. FINAL ACCEPTANCE OF THE TOTAL PAVEMENT INSTALLATION SHALL BE SUBJECT TO THE TESTING AND CHECKING REQUIREMENTS CITED ABOVE.
6. ALL MATERIAL AND CONSTRUCTION SHALL CONFORM TO THE CITY OF MILWAUKEE CODE... GENERAL NOTES AND SPECIFICATIONS, THE MORE STRINGENT SHALL TAKE PRECEDENCE.

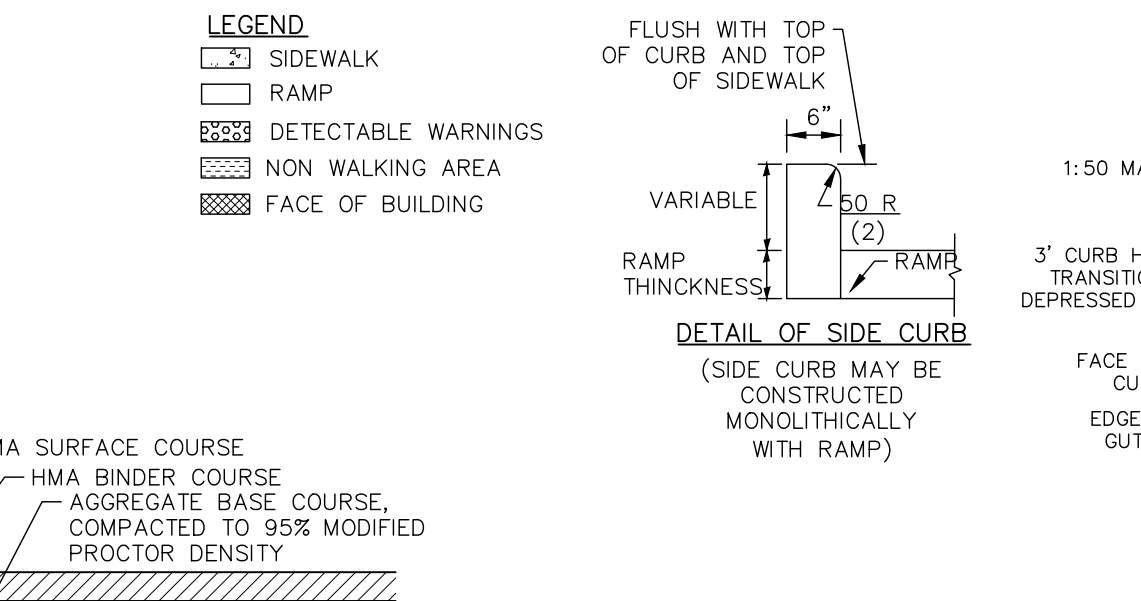


HEAVY DUTY CONCRETE PAVEMENT SECTION

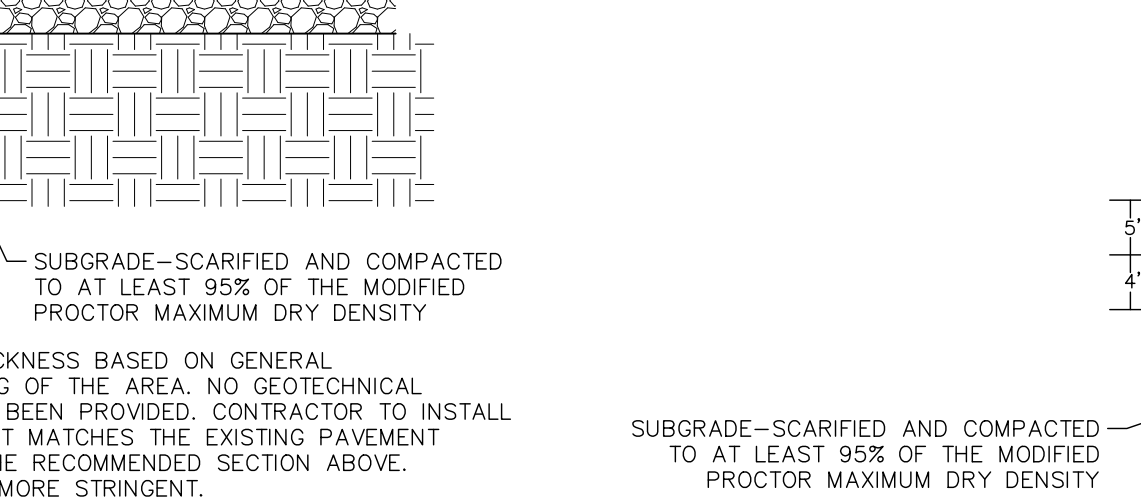
N.T.S.

GENERAL NOTES:

- 1. THE MAXIMUM SLOPE OF THE SIDE FLARE FOR TYPE B RAMPS SHALL BE 1:10; HOWEVER, IF THE WIDTH OF THE LANDING AREA BETWEEN THE TOP OF THE RAMP AND ANY OBSTRUCTION IS LESS THAN (4'-0") THEN THE MAXIMUM SLOPE SHALL BE 1:12.
2. ALL SLOPE RATIOS ARE EXPRESSED AS UNITS OF VERTICAL DISPLACEMENT TO UNITS OF HORIZONTAL DISPLACEMENT (V:H).
3. ALL DIMENSIONS ARE IN INCHES UNLESS OTHERWISE SHOWN.
4. RAMP DETAILS TO BE COORDINATED PER VILLAGE REQUIREMENTS.
5. DETECTABLE WARNINGS SHALL CONSIST OF TRUNCATED DUMPS WHICH CONTRAST VISUALLY WITH ADJOINING SURFACES...
6. CONTRACTOR TO SELECT ADA DETECTABLE WARNING PERMEABLE PAVERS OR CONCRETE WITH ADA DETECTABLE WARNING DOMES FOR RAMPS.

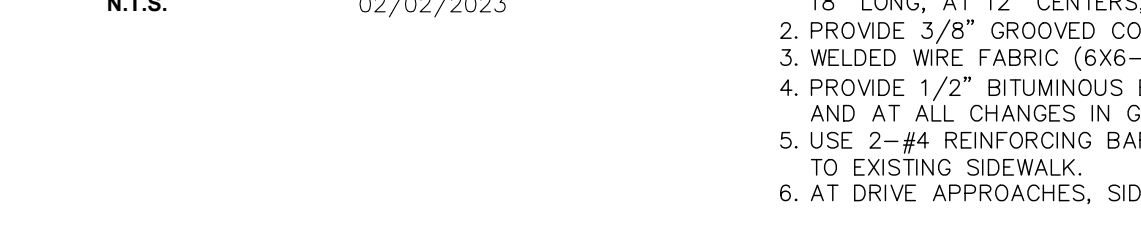


DETAIL OF SIDE CURB



STANDARD DUTY ASPHALT PAVEMENT SECTION

N.T.S. 02/02/2023



CONCRETE SIDEWALK

N.T.S.



CONDUIT PURPOSE, SIZE, AND RUN

SCALE: N.T.S.



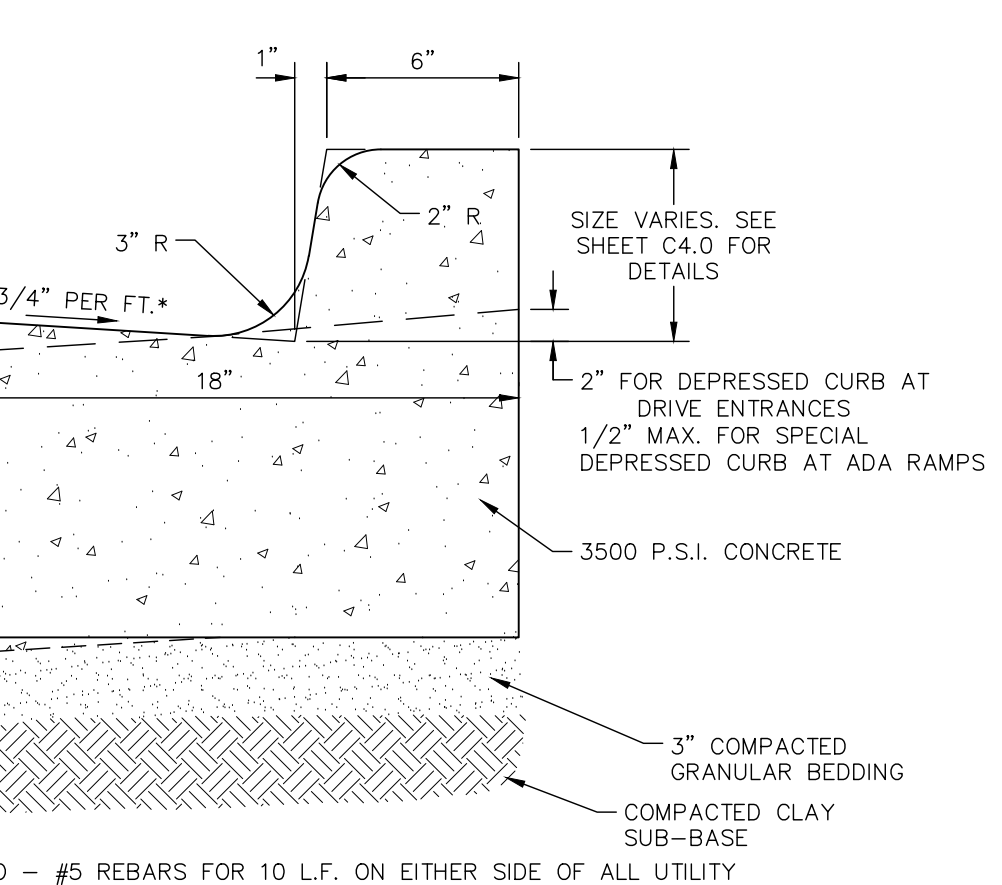
DRIVE-THRU SENSOR LOOP LAYOUT

SCALE: N.T.S.



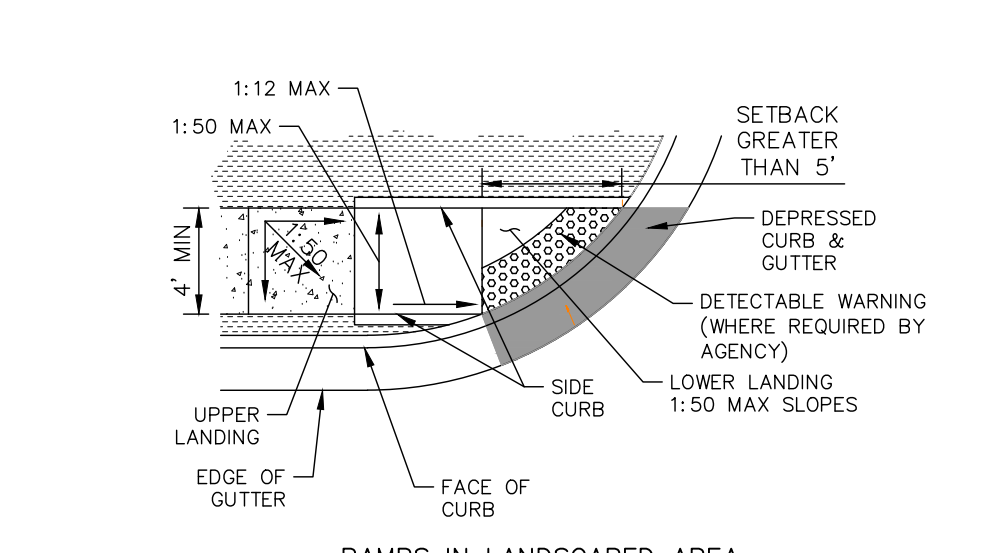
DRIVE-THRU SENSOR LOOP SECTION

SCALE: N.T.S.

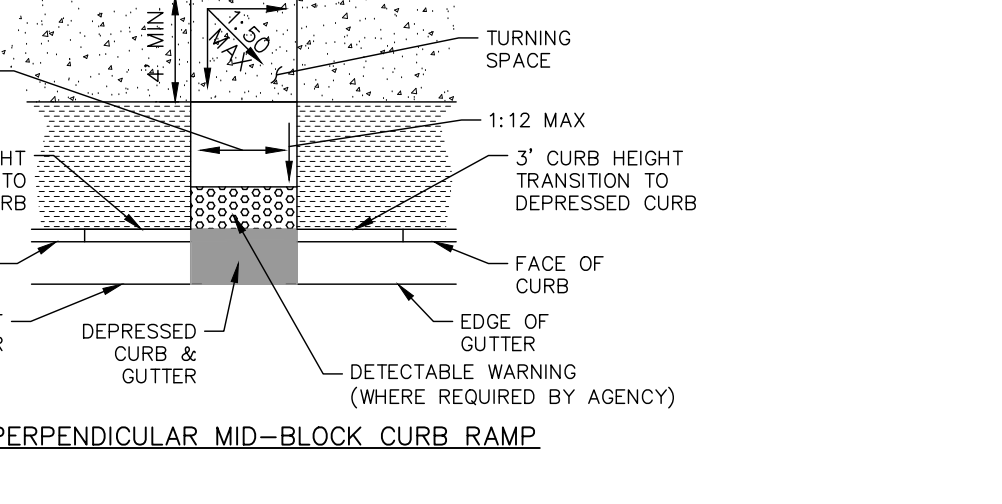


CURB & GUTTER

N.T.S.

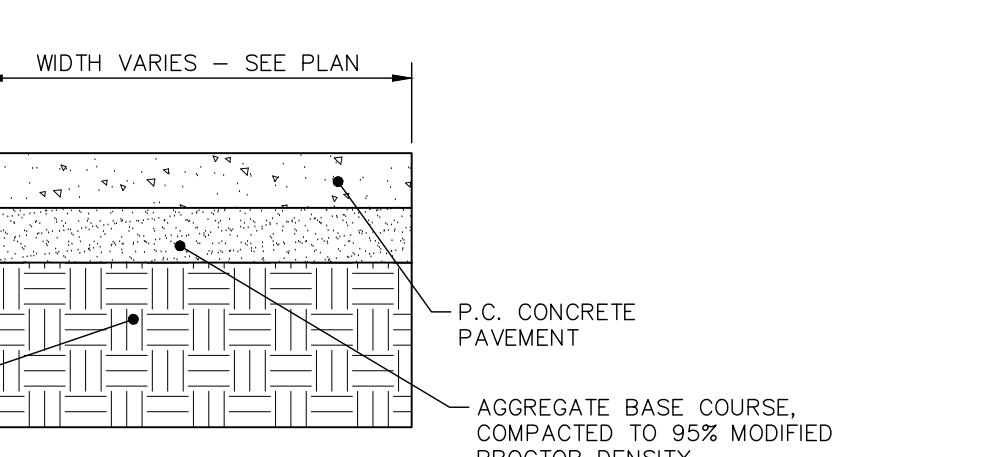


PERPENDICULAR MID-BLOCK CURB RAMP



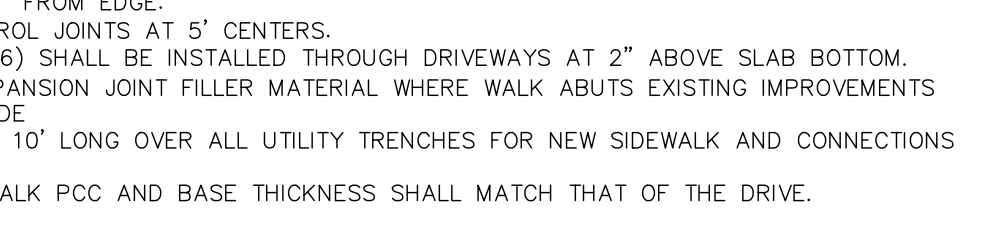
SIDEWALK AND SIDEWALK RAMPS

N.T.S.



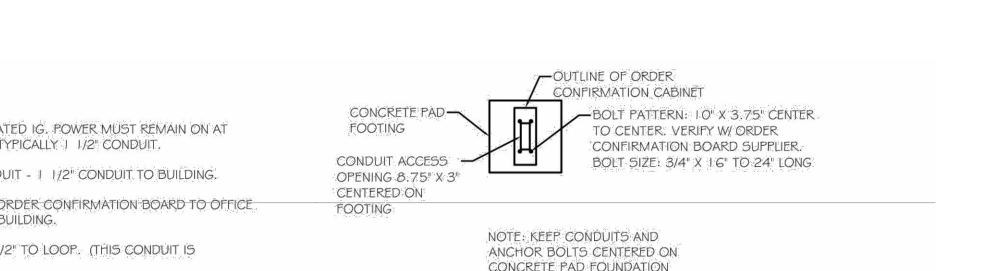
CONCRETE SIDEWALK

N.T.S.



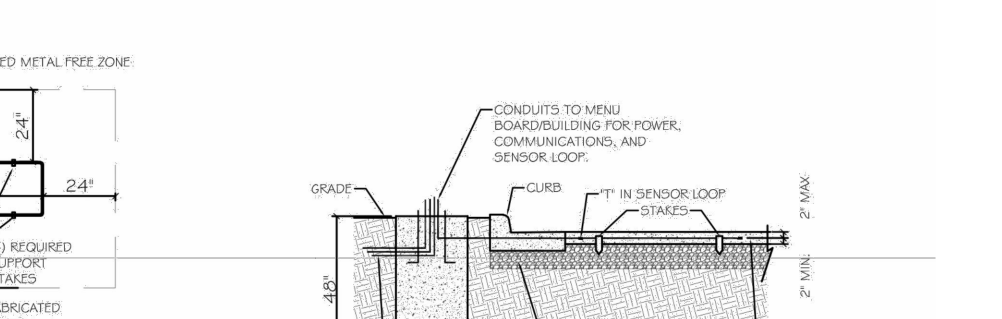
DRIVE-THRU SENSOR LOOP LAYOUT

SCALE: N.T.S.



DRIVE-THRU SENSOR LOOP SECTION

SCALE: N.T.S.



DRIVE-THRU SENSOR LOOP SECTION

SCALE: N.T.S.



DRIVE-THRU SENSOR LOOP SECTION

SCALE: N.T.S.

Scale: AS NOTED
DESIGNED BY: BWH
DRAWN BY: ANA
CHECKED BY: JPM
Kimley-Horn & Associates, Inc.
11 WEST JACOBSON BOULEVARD, SUITE 1320
MILWAUKEE, WI 53212
CULVER'S
575 WILYATON AVE
MILWAUKEE, WI 53207
GENERAL NOTES AND DETAILS
ORIGINAL ISSUE: 02/08/2023
KHA PROJECT NO. 168974004
SHEET NUMBER C1.0

WARNING: VERIFY WITH SITE PLAN AND FRANCHISEE IF PODS ARE TO BE INCLUDED IN PROJECT.

- GENERAL NOTES:**
- POD PROTOTYPE DRAWINGS ARE TO GIVE SCHMATIC DESIGN INTENT. FRANCHISEE IS RESPONSIBLE FOR ANY PERMITTING OR APPROVALS WITH CITY.
 - CONTRACTORS ARE RESPONSIBLE TO MAKE SURE DESIGN AND CONSTRUCTION IS COMPLIANT WITH LOCAL CODES AND CONSTRUCTION PRACTICES.
 - EXTERIOR FINISHES OF POD TO MATCH FINISHES USED ON RESTAURANT LOCATION!

DOOR SCHEDULE

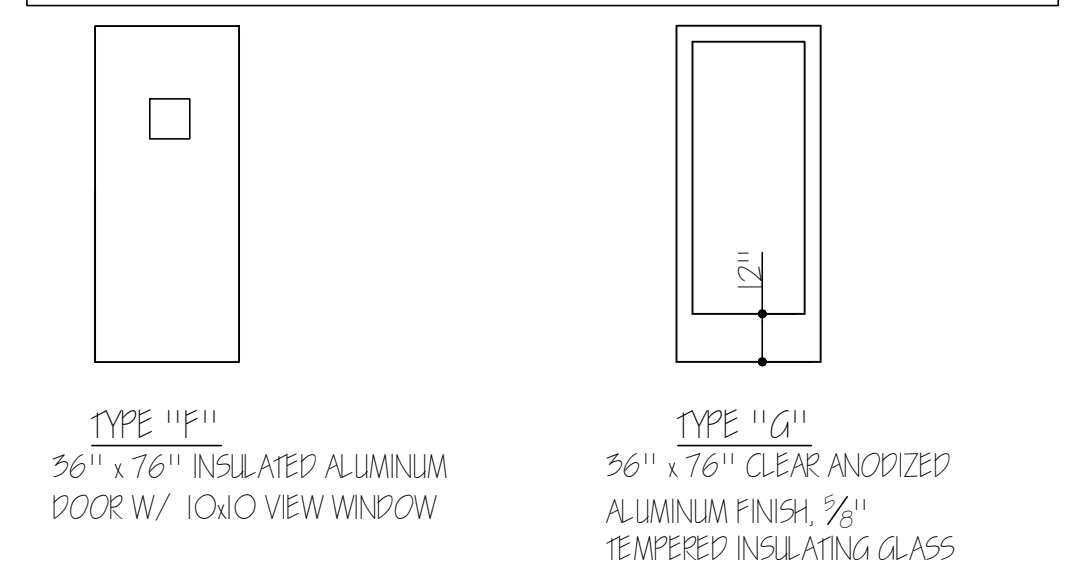
DOOR:					FRAME:		HARDWARE GROUP	NOTES
NO.	WIDTH	HGT.	MAT'L	TYPE	MATERIAL	TYPE		
1	36"	76"	ALUM	F	ALUM	E	1	
1a	2 @ 36"	76"	FG	G	ALUM	E	2	

NOTE: ONLY 1 DOOR PER POD. REF SITE PLAN AND POD PLAN TO VERIFY IF DOOR 1 OR 1a IS USED.

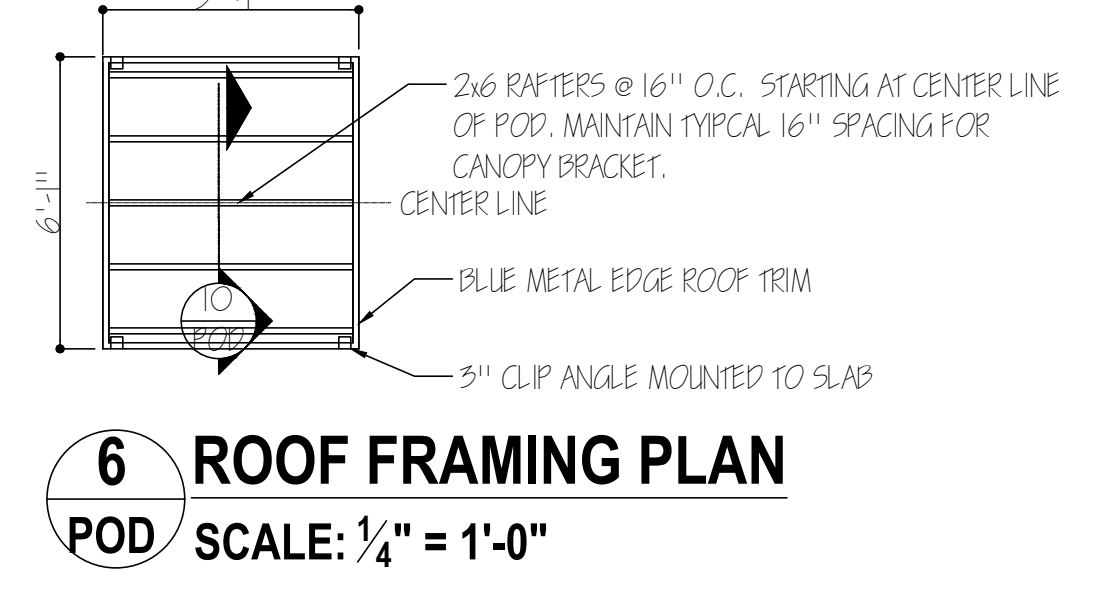
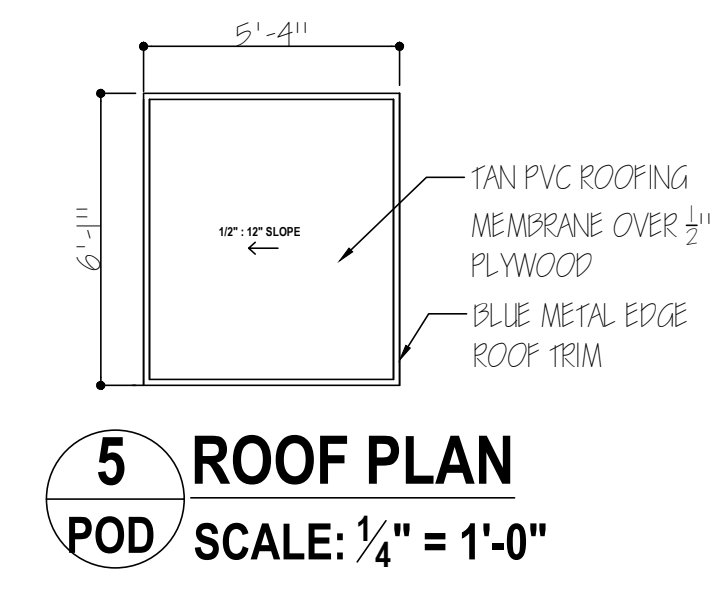
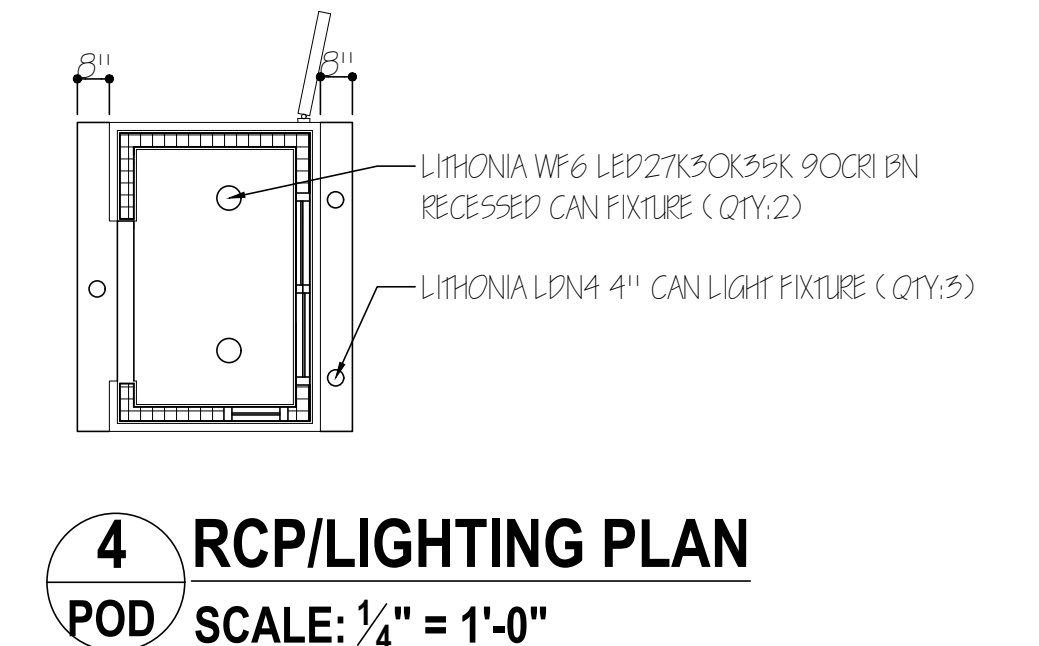
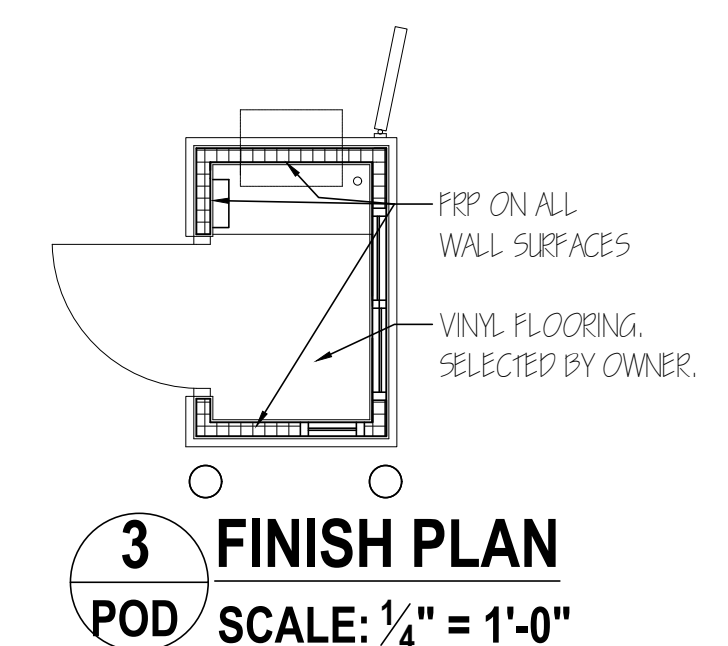
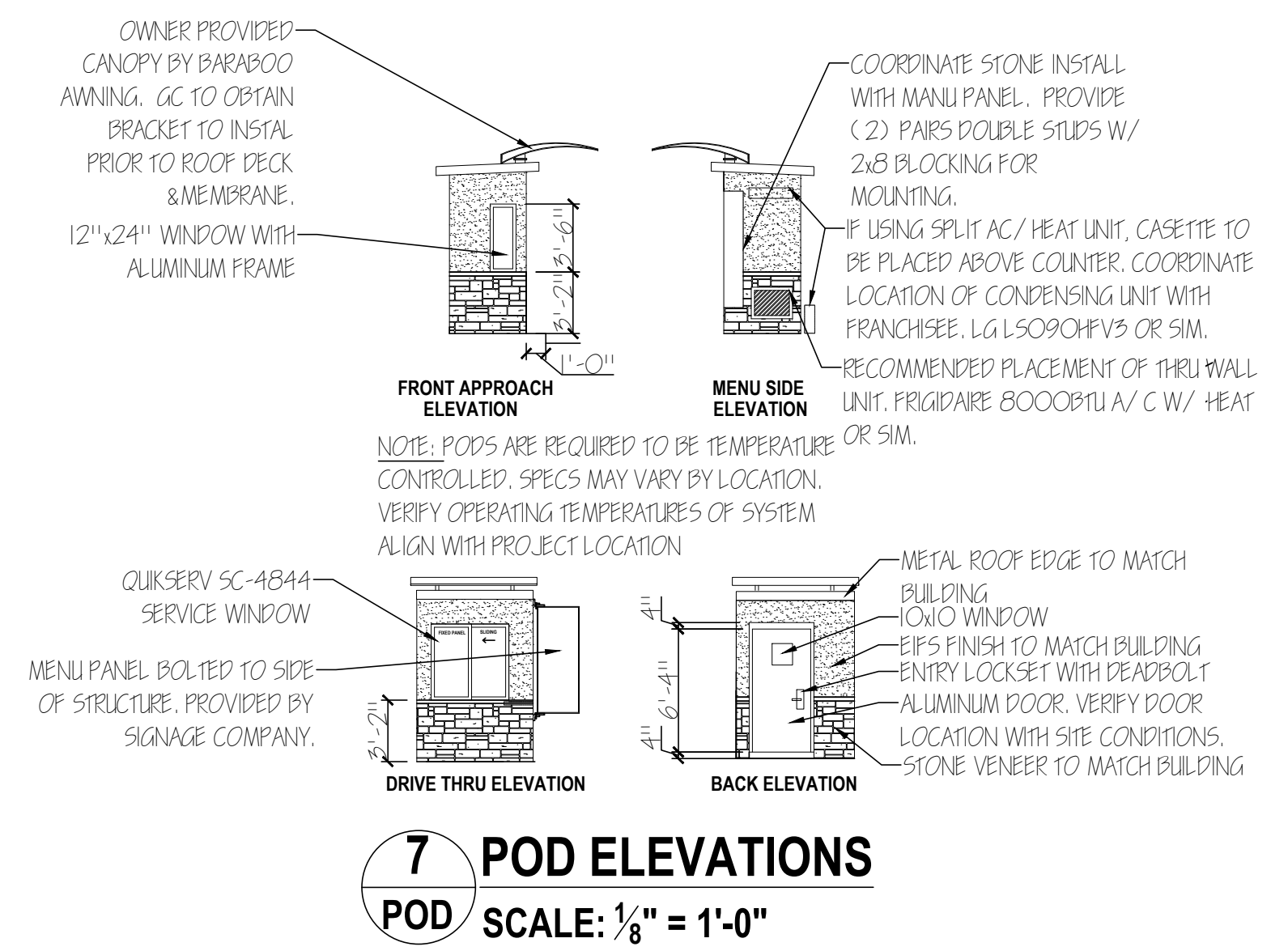
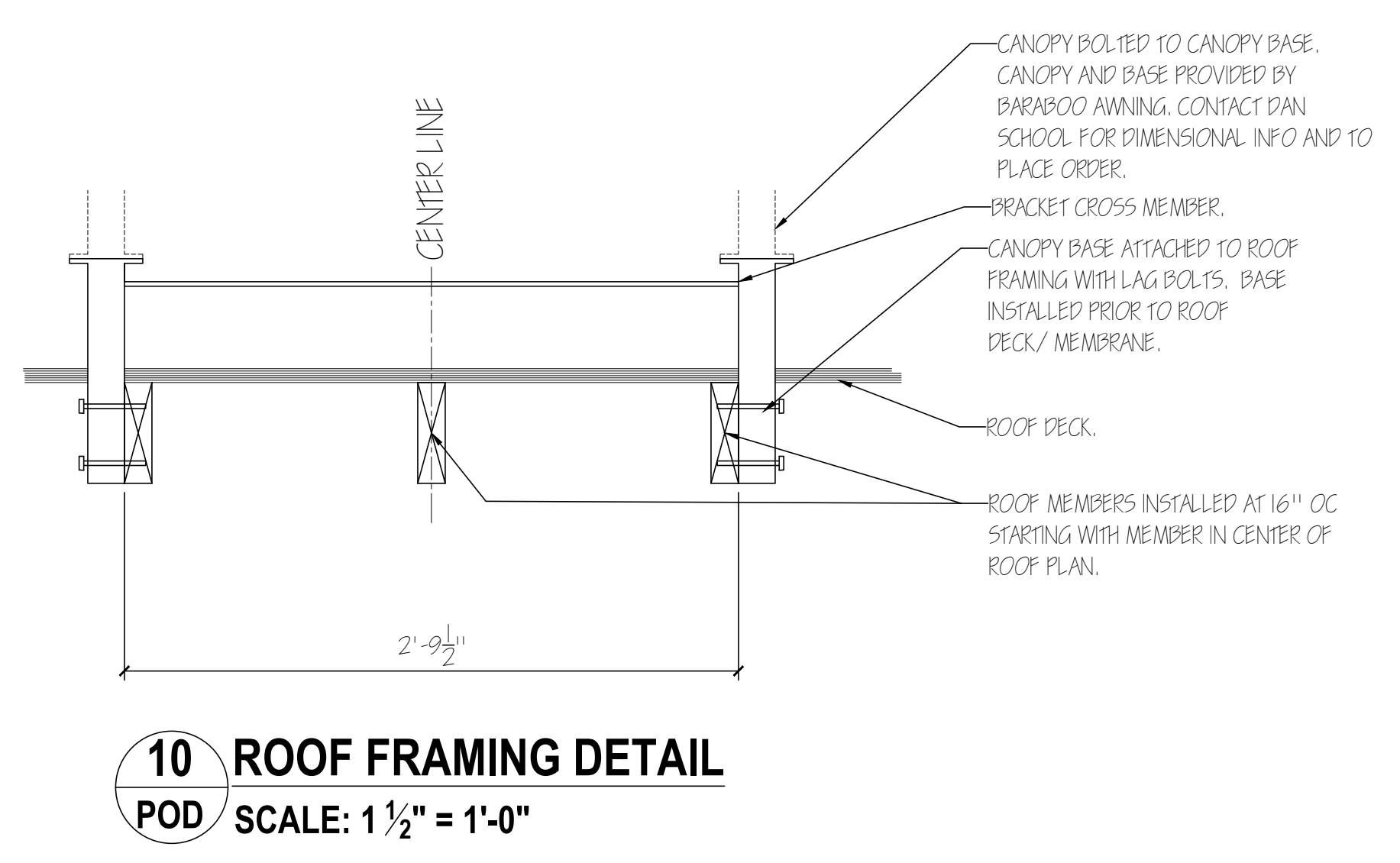
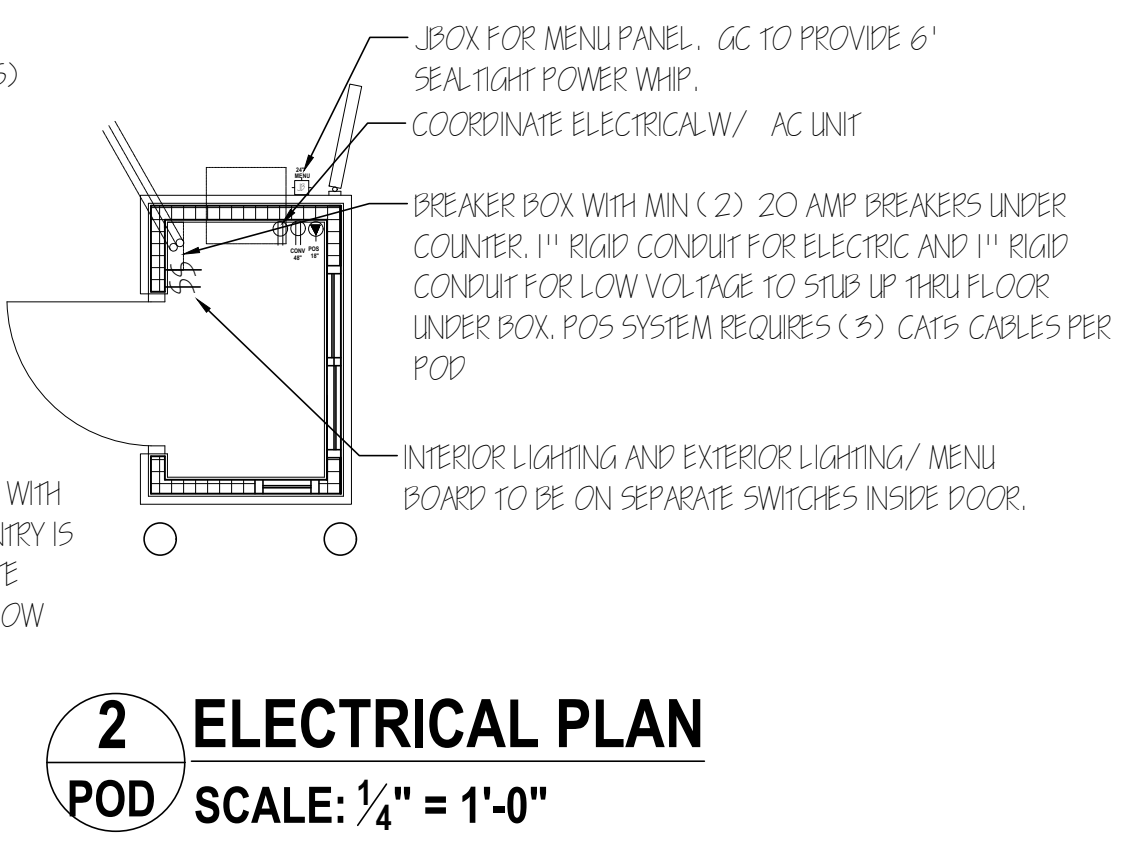
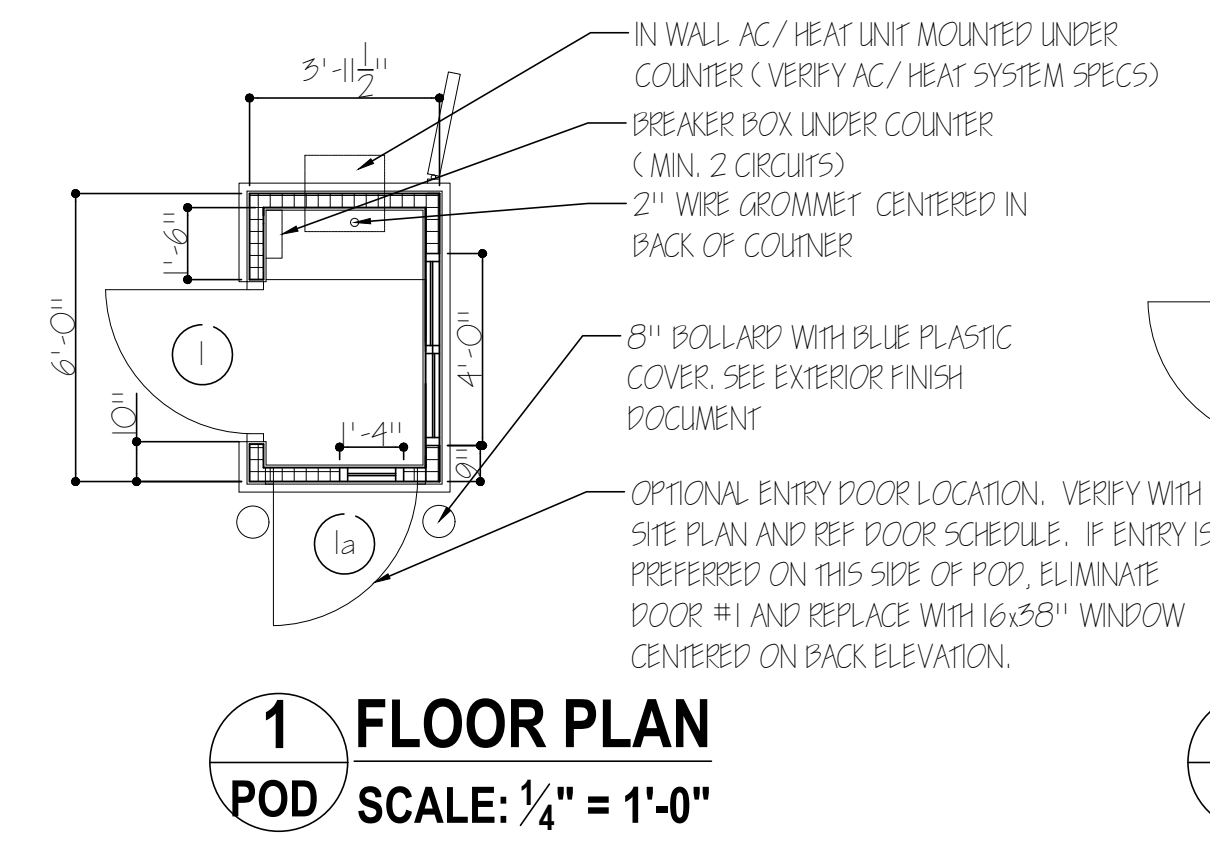
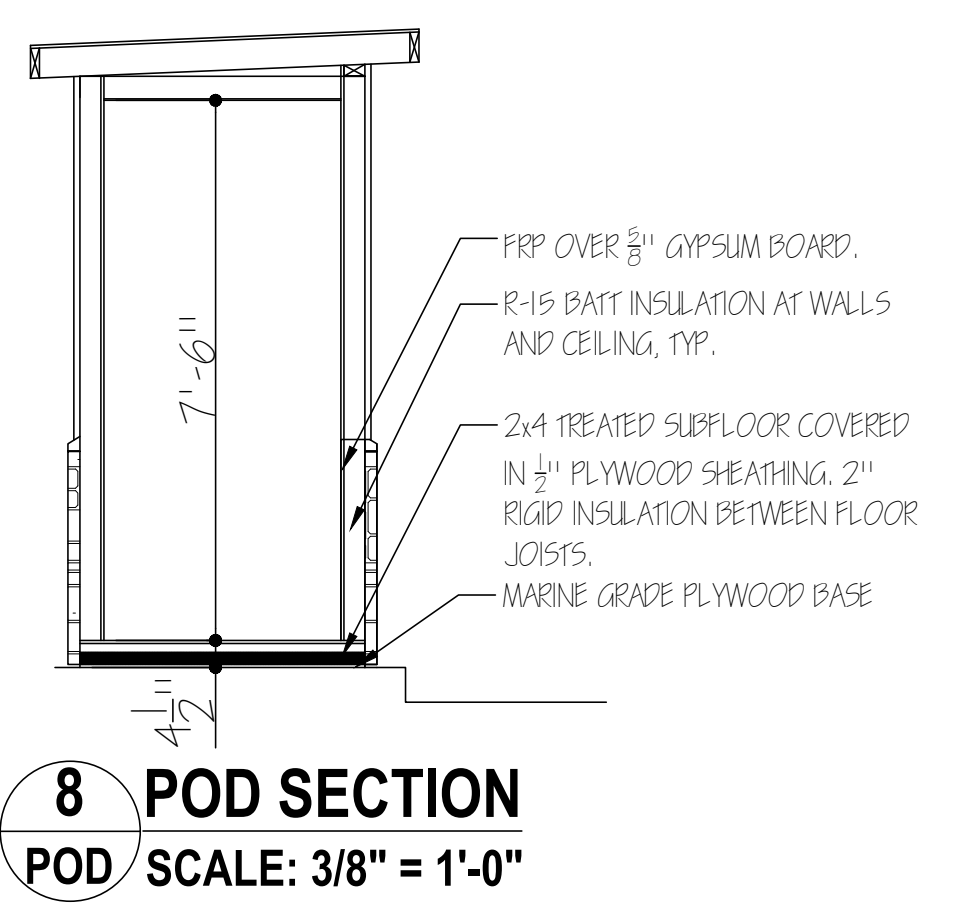
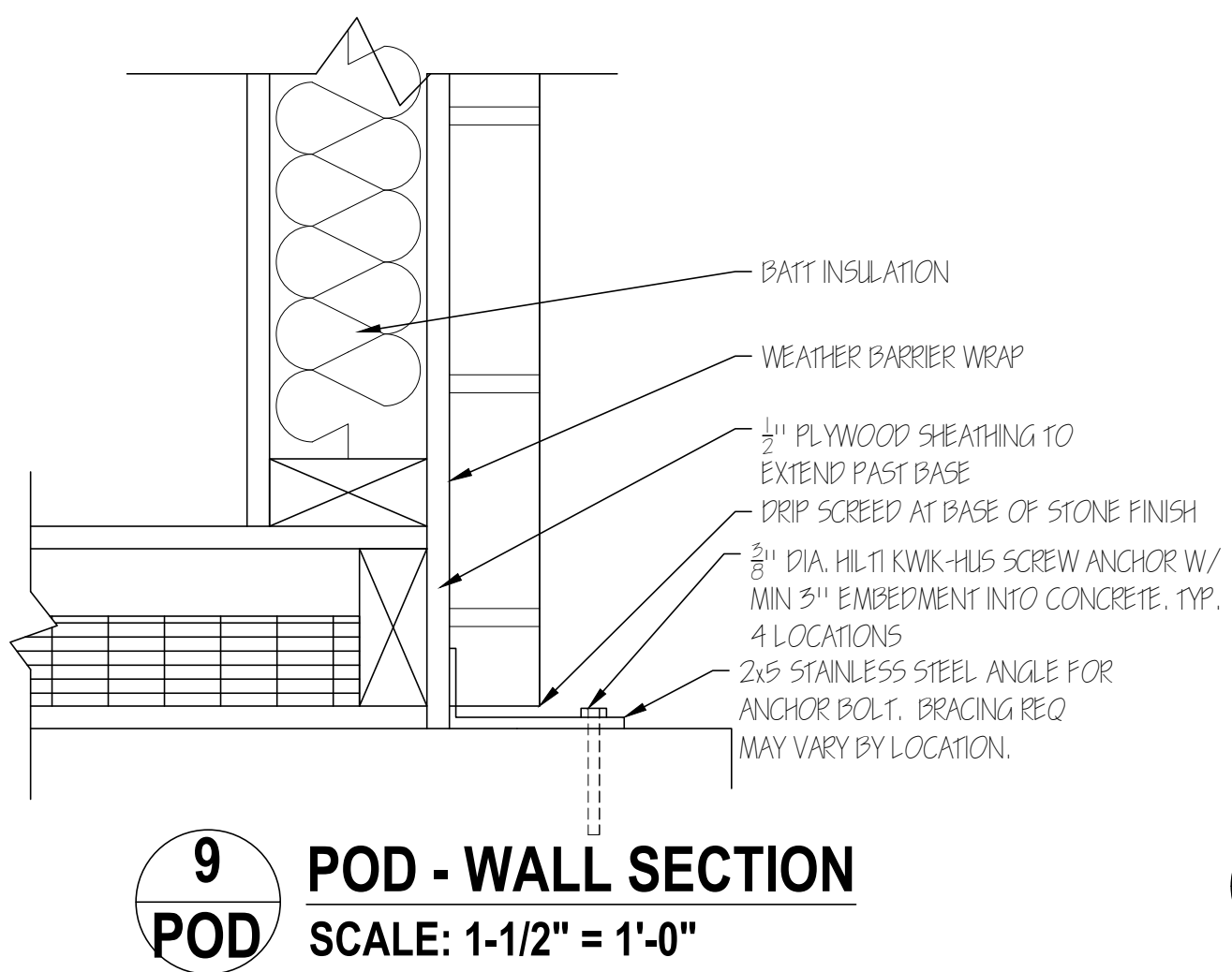
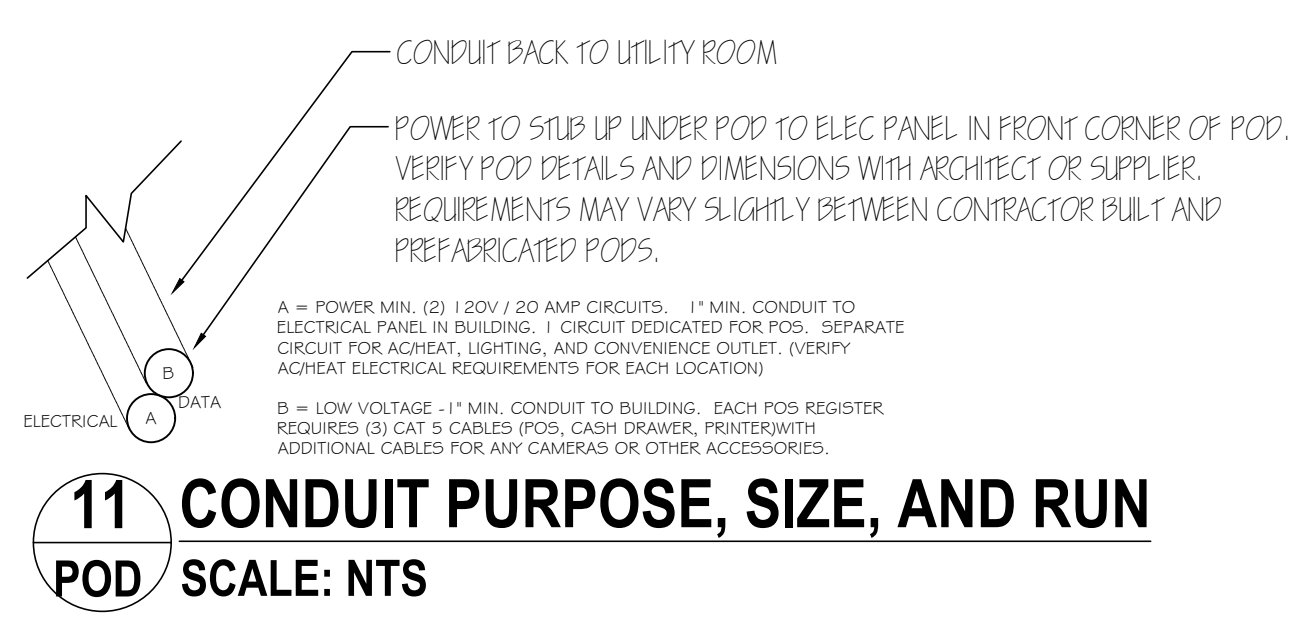
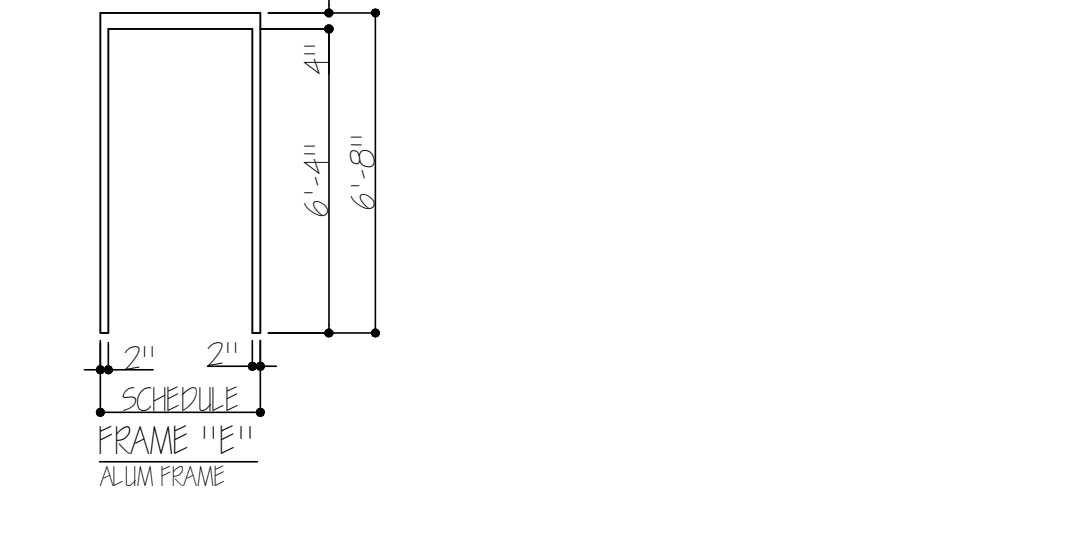
GROUP #	DOOR	QTY	DESCRIPTION	CATALOG #	FINISH	MFG
POD ENTRY DOOR	1	1	CONT. HINGE	224HD	628	IVES
	1	1	STORE RM LEVER SET	NDBORD RHO	628	SCHLAGE
	1	1	LOCK GUARD	LG12	630	IVES
	1	1	SURFACE CLOSER	4111 SHCLSH	689	LCN
	1	1	ARMOR PLATE	8400 36"x2"LDW B-CS	630	IVES
	1	1	GASKETING	429A	A	ZERO
	1	1	DOOR SWEEP	328AA	AA	ZERO
	1	1	DOOR SWEEP	39A	A	ZERO
	1	1	THRESHOLD	65A-MSLA-10	A	ZERO
	1	1	RAIN DRIP	142A	A	ZERO
ALTERNATE GLASS ENTRY DOOR	2	1a	CONT. HINGE	112HD	628	IVES
	1	1	DEAD BOLT LOCK			
	1	1	RIM CYLINDER	20-057	628	SCHLAGE
	1	1	PUSH AND PULL SET			
	1	1	CH STOP	1005	630	GLYNN
	1	1	SURFACE CLOSER	4111 EDA	689	LCN
	1	1	MOUNTING PLATE	4110-1B	689	LCN
	1	1	BLADE STOP SPACER	4110-61	689	LCN
	1	1	DOOR SWEEP	39A	A	ZERO
	1	1	THRESHOLD	625A-MSLA-10	A	ZERO

*NOTE - PERIMETER WEATHER STRIPPING BY DOOR/ FRAME MFG.

DOOR TYPES



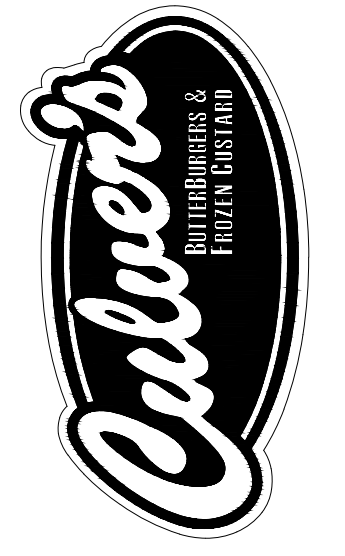
FRAME TYPES



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By	
Date	
Revision	

Culver Franchising System, LLC
1240 Water Street
Prairie du Sac, WI 53578
608-643-7980



DRIVE-THRU ORDERING POD PROTOTYPE PLANS CULVER FRANCHISING SYSTEM, LLC

Contents:
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Metro L - 2023
R. KOTILA
Jan. 1, 2023

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POD-1